

**CITY OF MILWAUKIE
PLANNING COMMISSION
MINUTES
Milwaukie City Hall
10722 SE Main Street
TUESDAY, December 9, 2014, 2014
6:30 PM**

COMMISSIONERS PRESENT

Sine Bone, Chair
Wilda Parks, Vice Chair
Scott Barbur
Greg Hemer
Shaun Lowcock

STAFF PRESENT

Denny Egner, Planning Director
Li Alligood, Senior Planner
Vera Kolias, Associate Planner
Peter Watts, City Attorney

COMMISSIONERS ABSENT

Shannah Anderson
Gabe Storm

1.0 Call to Order – Procedural Matters*

Chair Bone called the meeting to order at 6:30 p.m. and read the conduct of meeting format into the record.

***Note:** The information presented constitutes summarized minutes only. The meeting video is available by clicking the Video link at <http://www.ci.milwaukie.or.us/meetings>.*

2.0 Planning Commission Minutes

3.0 Information Items

Denny Egner, Planning Director, noted that the Monroe Street Neighborhood Greenway Concept Plan project open house on December 3rd was well-attended and commended Vice Chair Parks on chairing the meeting. He added that there was a split opinion on the project so far but there was good feedback received. The consultant team and advisory committee would work on a draft concept plan to bring back to the public in the next few months.

Vice Chair Parks stated that it was a good meeting and added that a number of attendees lived elsewhere in Milwaukie but saw the need for improvements along Monroe St.

4.0 Audience Participation –This is an opportunity for the public to comment on any item not on the agenda. There was none.

5.0 Public Hearings

- 5.1 Summary: Reliable Credit Parking Lot
Applicant/Owner: Sisul Engineer/ L&B Holzman LLC
Address: 10605 SE Main St
File: DR-14-07
Staff: Vera Kolias

Chair Bone called the hearing to order and read the conduct of quasi-judicial hearing format into the record.

Commissioner Barbur stated that, as a member of the Historic Milwaukie Neighborhood District Association (NDA), he declared an ex parte contact because the applicant had attended the July 14, 2014, NDA meeting and discussed the demolition of the building. However, he did not participate in the discussion and abstained from the vote of the NDA.

Commissioner Hemer declared that he spoke with Val Ballestrem, a Design and Landmarks Committee (DLC) member, and had discussed the design review meeting. The information discussed was no different than what was reflected in the DLC minutes provided in the meeting packet.

Vera Kolias, Associate Planner, presented the staff report via PowerPoint. She oriented the Commission to the project site and to the area zoning and uses. She reviewed the proposal to construct a surface parking lot for employees which would include landscaping and additional lighting. She noted that a surface parking lot was a permitted use in this location. The proposal required Downtown Design Review but demolition of the existing building only required a permit issued by the Building Official. A revised site plan was submitted at the DLC meeting and included a seat wall along Main St to the corner of Scott St and more ornamental light fixtures.

Ms. Kolias reviewed the approval criteria of compliance with Title 19 and with Downtown Design Guidelines. She identified key issues for the Commission to address with regard to the Downtown Design Guidelines:

- Did the proposed design reinforce Milwaukie's sense of place and provide human scale to the pedestrian environment?
 - Ease of access to the building and sidewalks was addressed in the proposal.
 - The landscaping, seat wall, and lighting provided enclosure, human scale, and safe and comfortable places where people could stop to sit or rest.
 - The DLC recommended a façade wall that would mimic a storefront to provide the street wall, and for the applicant to submit two alternative designs upon submittal of the development review applications.
 - The parking lot as proposed did not provide enough unique qualities or interest to meet the character guidelines.
 - **Ms. Kolias** displayed examples of alternative edge treatments of structural wall facades as recommended by the DLC.
- Was the newly proposed lighting consistent with the recommended ornamental style?
 - **Ms. Kolias** displayed the original proposed lighting, which was the same as the existing lighting in the Reliable Credit Parking lot, and the ornamental lighting recommended by the guidelines, which the revised proposal included.
 - The DLC had recommended a combination of the ornamental and utilitarian lighting; the ornamental would be along Main St and the utilitarian lighting would be allowed to provide additional lighting elsewhere in the parking lot.

Ms. Kolias noted that an additional requirement for existing office uses that required a minimum of 44 vehicle spaces and 4 bicycle spaces. A condition for the application was written to address this issue. She reviewed the proposed conditions recommended by the DLC. Several comments submitted were in opposition to the demolition of the building. Staff recommendation was that the proposal complied with the standards and guidelines as conditioned. She reviewed the decision-making options.

Ms. Kolias answered questions of the Commission.

- The DLC recommendation was for a structural wall that would include seating that would satisfy the requirement for a place for pedestrians to sit and rest
- The bicycle parking requirement would apply to the entire site, but should be located near the building.
- There would be a total of 34 parking spaces for the entire site, and the number of required vanpool/carpool parking spaces was included.

Chair Bone called for the applicant's testimony.

Lee Holzman, owner of Reliable Credit and subject property, 2542 SW Hillcrest Dr, Portland, was concerned about available parking in the area around his business once the block to the south at Main St and Harrison St was developed by Metro and the City of Milwaukie. He was doing what he could to minimize that impact for his customers and employees. He noted that there was dialogue with the City to explore other options but those options would need to be long-term solutions. If the parking lot was in fact constructed, he would ensure it would look as pleasing as was possible.

Tom Sisul, Sisul Engineering, 375 Portland Ave, Gladstone, addressed the recommendation of the storefront façade and noted that if it was required, the ornamental lighting would be on the backside of the wall and create shadows and block light. There had only been a week to review the DLC's recommendations but his suggestion was to propose the original seat wall and include 4 ft columns that would not block light or sightlines. There was no conflict with the remaining conditions and was willing to adhere to the recommendations.

The applicants answered questions of the Commission.

- Of the 21 current spaces, there were only 10 spaces exclusively for employees. By adding the 13 spaces with this proposal, it would preserve the current customer parking.
- The parking lot would be available for use after business hours; people park in the current lot for the farmers market, etc.
- Regarding the narrow lot, most of the measurements for the proposal were near minimum. The intent of the proposed lot was for employee parking whereas customer parking would be near the entrance to the building. Therefore, there may be less pedestrian frequency in this lot.
- Electric vehicle parking had not been considered but that could be retrofitted later on.
- The shoebox light style was proposed in order to reduce light pollution into the residential units across the street. The recommended ornamental fixtures spread light wider and therefore may result in more shadows. If the shoebox fixtures were used alongside the recommended wall, they would serve to light the parking stalls only rather than provide light along Main St.
- Regarding alternative transportation of the employees, there was one bike rider, one bus rider, and one employee that intended to take light rail once service began.

Mr. Egner clarified that the Commission had the final decision authority; and the DLC recommendation was for the development review return to the Committee but it was up to the Commission to decide if that should occur.

Chair Bone called for public testimony.

Neutral:

Denise Baker, 10606 SE Main St, appreciated the recommended modifications to the proposal, and the applicants for their open communication. She understood that the owner's concerns; however, she saw the amount of people that did business at Wind Horse Coffee, Roger & Ives, and Casa de Tamales. The owner had rights but the guidelines and community wanted retail businesses, not a wall that mimicked retail storefronts. Forward planning needed to be considered and perhaps there were other options to be considered. A plain parking lot would never make a downtown Main St vibrant.

Charles Mayes, Casa de Tamales, respected the owner and his right to build the parking lot but hoped Mr. Holzman would reconsider. He noted that Casa de Tamales drew most of its customers from outside of Milwaukie, so was a big draw for downtown. Since the proposed parking lot would be available after hours, the owner could provide electricity, water, and gas hookups lot so that it could be used in the future for such things as food vendors. There would be a lot of people that would be put out of work if the parking lot was built. With regard to the number of employees of the owner that drove single-passenger cars, he hoped that Mr. Holzman could better incentivize carpooling, public, or alternative transportation for his employees.

In Opposition:

Roger Thompson, 10606 SE Main St, lived across the street from the property and worked in a business located in the buildings to be demolished. He noted that 90% of the businesses patrons came from outside of the district, which therefore brought customers into the neighborhood that would frequent other businesses. He was a long-time resident and was very encouraged about increased retail on Main St and felt that it was what the downtown really needed. He was concerned as to why employees could not walk to work from other nearby and free parking areas. There were a lot of positive things happening for Milwaukie so removing viable retail on Main St would be detrimental. He encouraged the Commission to mediate with Mr. Holzman to find solutions that would save retail businesses in downtown Milwaukie.

Cheree Heppe, 10606 SE Main St, had lived in Milwaukie since 2012. She asked what kind of people it took to make a community; a resident considered the wellbeing of the community as a whole. Community membership was more than just the exercise of rights; it included the mindful application of responsibility and the best interest was not always served by dollars and cents, but by good will. Not many towns retain specialty businesses like ethnic eateries, amazing coffee shops, and trendy art shops. She assumed most of the applicant's employees were sighted and drove; she stated that she was blind and commutes by bus into Portland and walks 10 blocks to work every day. The parking lot across Harrison St seemed to be a viable option for employee parking and seemed to have availability during the week. Removing part of the charm and uniqueness of Milwaukie and replacing it with a parking lot removed business activity and would spoil livability, viability, and flavor of the town. Sometimes it was not about what can be done but what should be done.

Stephanie Hower, 4185 SE Howe St, noted she was new resident and homeowner in Milwaukie. She saw that the downtown was growing and rebuilding and chose Milwaukie because she saw the direction it was going in. She disagreed that a parking lot that resulted in

the loss of local businesses contributed in any way to a “sense of place” and therefore did not meet the design guidelines. She commuted by public transit and foot year-round and across busy streets and, along with her, many of her coworkers did not have assigned parking. She noted that her employer supported, encouraged, and funded use of alternative transportation. She addressed the applicant to not be a villain of downtown Milwaukie by closing Main St business but to be a hero by working with the City to help create a safer pedestrian experience. She asked the applicant who they expected to be their customers in a growingly-vacant downtown.

Ben Rousseau, 3264 SE Lake Rd, noted he moved to Milwaukie 4 years ago and was drawn by the vibrancy of the farmers market, First Friday event, and other community attractions. He and his family frequented businesses downtown including Wind Horse Coffee, and added that these types of places were important for the community. A parking lot would destroy a sense of place and took the city away from the goals that it was working toward with the Moving Forward Milwaukie project, and would set a negative tone for attracting new businesses to downtown.

Robert Morgan, 10554 SE Main St, noted that he was in opposition to the proposal but believed in rights of ownership as well. He understood the struggles Mr. Holzman had with parking, and added that the government had not been accommodating with regard utilizing to the parking lot across the street from Mr. Holzman’s business and other downtown businesses. However, a façade of demolished businesses would appear like a tombstone. He asked the Commission consider other solutions to the parking issue.

James Knights, 10987 SE 28th Ave, frequented the businesses to be demolished. He was in support of all of the comments given so far. He noted he would be shocked if there was not another solution to satisfy both parties other than removing businesses.

Chair Bone asked for questions from the Commission.

Commissioner Hemer asked how long the decision would stand for if the Commission made a decision.

- **Ms. Kolias** replied that the permit applications would need to be filed within two years of the decision and completion of construction would need to be completed within four years.
- **Commissioner Hemer** verified that the approval then would be "grandfathered in" once the code changes under the Moving Forward Milwaukie project became effective.

Chair Bone closed the public testimony.

The Commission deliberated.

Vice Chair Parks noted that the Commission was charged with looking at the code and how the proposal met the approval criteria, and acknowledged that the property owner had the right to demolition. However, it was both difficult and interesting to listen to the different perspectives on this proposal. As a citizen, she hoped that whatever the outcome of the meeting, the discussions would continue to find other solutions that could satisfy the heart of Milwaukie and the business interests of Milwaukie. She liked the idea of installing utilities in order to provide opportunity for community use. Regarding design, she was more in favor of a structural wall rather than a low wall around the parking lot.

Chair Bone agreed about the structural wall. She referred to the discussions within the Moving

Forward Milwaukie project with regard to urban design and the street wall, to maintain visual interest for the pedestrian. She also agreed that the proposal was difficult to consider; the Commissioners themselves frequented the businesses involved. There was little the Commission could do with the code as it was written today. She commended the applicant for searching for a solution for his employees, but she hoped that he would continue communication with the Mayor and other staff to find other solutions. She commuted and walked to work herself, and recognized there was a safety problem with the intersection at Main St and Harrison St. However, it was unfortunate that these businesses may be lost in the community.

Commissioner Barbur noted the question of the structural wall design element. The seat wall with seating for the pedestrians was important; could the two be combined in order to satisfy both elements. He was also concerned about the safety issues that may come with a wall with regard to lighting and hiding spaces.

Commissioner Hemer noted that the proposal, as far as parking lots, included appealing elements. A wall would hide much of that, including the ornamental lighting elements; a solution could be wall-mounted lighting on both sides of the wall. He asked about the time frame of the approval because he understood that the property owner was planning ahead; the current parking lot being used was an opportunity site for development under the MFM [Moving Forward Milwaukie] project. Demolition and construction of the parking lot would be costly, so his speculation was that the owner was protecting his options. He believed the proposal was approvable by the code and the right of the property owner. Creating a condition to consider including utilities would be ideal.

Commissioner Lowcock noted from a citizen standpoint, he agreed with the public testimony and concern about the loss of business in downtown for a number of reasons, particularly with the goals of the MFM project in mind. However, the Commission was only tasked with reviewing if the proposal met the approval criteria, and it did. He agreed with the safety concerns about a structural wall. He was in favor of providing utilities to the lot in order to facilitate food carts, etc. He recognized that the property owner was being a sound businessman and protecting his options for the future. He hoped that the hearing process would encourage the owner to keep the dialogue with the City open to find solutions in order to keep the businesses alive.

Vice Chair Parks appreciated the concerns about the safety regarding the structural wall.

Mr. Egner reminded the Commission that the DLC recommendation included that the application return to the DLC to review the final design elements once permits were to be submitted. He noted that the Commission could condition that the DLC address the lighting concerns and safety issues. There was flexibility for the Commission.

Chair Bone noted the specific condition 4.C to amend to require lighting and transparency.

Vice Chair Parks noted that the applicant had stated they were willing to work with the community to make the parking lot as fitting as possible and that they were aware of the concern.

Mr. Watts noted that there were a few decision points involved; first, did the Commission want to require a wall; secondly, did they want a seating-height wall or a storefront height wall; and lastly, did they want to send it back to the DLC for final design review.

Mr. Egner clarified that the DLC recommended a wall with storefront openings that included seating areas.

Vice Chair Parks agreed that sending the final design elements back to the DLC was a good condition.

It was moved by Commissioner Hemer and seconded by Vice Chair Parks to approve land use application DR-14-07 for 10605 SE Main St with the findings and conditions as amended by Condition 4A to include "and shall light the sidewalk and the parking lot". The motion passed unanimously.

Commissioner Hemer asked how this approval related to the amendments that were involved with the Moving Forward Milwaukie plan and code amendments.

Li Alligood, Senior Planner, explained that the under current code, surface parking lots were prohibited within 50 ft of Main St south of Harrison St. The existing code treated the area north of Harrison St very differently. The Moving Forward Milwaukie draft code amendments would standardize those requirements to include that prohibition along Main St. She believed that was the reason this application came forward now; once the proposed amendments were approved, this type of application would not be approvable without a variance with high standards.

Commissioner Lowcock asked about the parking lots across from City Hall between Harrison St and Jackson St.

Ms. Alligood responded that the lots were known as the Texaco Site and was approved as a conditional use.

Mr. Egner noted that the Texaco Site was designated as an opportunity site and the City had been approached by developers with interest in the site. Staff would be discussing with City Council early next year about if they would like to move forward with a marketing program for the site. He was unsure what that process would look like, however, and there were some issues to work out if the site were to be developed. He noted that there was a request for the supplemental budget to include funding for an updated downtown parking plan.

6.0 Worksession Items

- 6.1 Summary: CPA-14-02 Moving Forward Milwaukie Downtown Plan and Code Amendments- Downtown & Riverfront Land Use Framework Plan
Staff: Li Alligood and Denny Egner

Li Alligood, Senior Planner, presented the staff report via PowerPoint and noted that this was the 11th and final worksession of this project. Tonight was to review the proposed amendments to the Downtown and Riverfront Land Use Framework Plan (Framework Plan).

The Commission had requested staff to provide additional information about incentivizing green building, what programs were in place in other similar communities, and what other certification programs were available. **Ms. Alligood** reviewed the results.

- Ashland and Dallas, OR, had density bonuses for residential projects that were Earth Advantage or LEED certified for a variety of green elements. Certification was verified by a third party.

- Earth Advantage and LEED certifications could be applied to residential, commercial, and mixed-use buildings.
- **Ms. Alligood** clarified that 'density' could mean more but smaller units, an additional story, or floor-to-area ratio (FAR).
- **Commissioner Hemer** was concerned about a height bonuses since many citizens were had issue with taller building heights in downtown.
 - **Ms. Alligood** clarified that the proposal was to reduce the building height in downtown to 3 stories but added that proposals for bonuses would be cumulative for green building and open space that could result in a 5 story building; that would be the maximum height in downtown south of North Main Village.
 - **Mr. Egner** added there could be 3 height or FAR bonus options for including residential, green building, or open spaces in the development, and could be cumulative up to an additional 2 stories.
- **Ms. Alligood** noted that she was seeking direction on which bonuses should be included since the draft proposals would be available to the public on Friday, December 12, along with notice of the public hearing.
- **Chair Bone** believed these incentives should be included in the draft proposals and the menu option would be the best method for up to an additional 2 stories. She acknowledged that maybe these bonuses were not aggressive, but hoped that it would make clear to developers that the community was interested in these features and types of development.
- **Ms. Alligood** confirmed the Commission's direction to include incentives for certifiable green building programs, and to present the options as a menu to choose up to 2 of 3 bonuses.

Ms. Alligood continued and presented the proposed amendments to the Downtown and Riverfront Land Use Framework Plan. She summarized the plan that was adopted in 2000 and noted the fundamental concepts about how downtown Milwaukie should function that included the use of anchors and attractors, emphasis on Main St as a healthy retail street, and the importance of connecting downtown to the river. The vision and concepts were implemented through the City's use, design, and development standards.

Ms. Alligood referred to the 2013 project Fresh Look Milwaukie: Downtown Road Map that reviewed the vision for downtown, with the resulting findings that the vision still reflected what the community wanted but the Framework Plan should be refreshed. Three adopted plans that helped to refine the vision and draft the proposed amendments were the Transportation System Plan, the Riverfront Park Master Plan, and the South Downtown Concept Plan. She explained how each plan's policies influenced the refreshed Framework Plan with regard to removal of the downtown transit center, final design and concepts of the Riverfront Park, and the defined character of the light rail station area and projects that would shape that area. Projects that have been completed that implement the vision since the Framework Plan's adoption included the North Main Village, Riverfront Park Phase I, the light rail alignment and station, restoration of Kellogg Creek (initiated) and Johnson Creek (completed). Current projects included planning for Kronberg Park, Riverfront Park Phase II, the Kellogg Bicycle and Pedestrian Bridge, and the Adams Street Connector. She displayed the existing and proposed fundamental concepts map for downtown, noting the importance of south downtown and Adams St and 21st Ave connections, renewed interest in McLoughlin Blvd and the connection to the Riverfront Park, and the concept of gateways at the north and south ends of downtown.

Mr. Egner noted that most of the conceptual illustrations were taken out of the Framework Plan as most of them were no longer applicable or unrealistic.

Ms. Alligood added that the Comprehensive Plan and Framework Plan were intended to be broad policy documents but included detailed schematic designs for individual sites that proved to be confusing and misleading because those designs were not the only way those sites could be developed. Staff proposed to remove those schematics to make the plans more clear in terms of concepts and policies rather than setting unrealistic expectations of what a concept may look like.

Commissioner Lowcock asked how the proposed dam removal affected the concepts for downtown.

- **Ms. Alligood** responded that the Kellogg-for-Coho initiative assumed the removal of the dam and restoration of Kellogg Creek to occur and the project was incorporated into the Framework Plan.
- **Mr. Egner** noted that the project was currently on hold since the dam removal had to be coordinated with a new McLoughlin Blvd bridge. There were a number of state and federal agencies involved with the project and so the regulatory aspect of the project was complex. There was funding for the restoration of the creek but not for the bridge at this time.

Chair Bone requested that the section titles be reconsidered to be more appropriate and indicative.

The Commission agreed that the proposed Framework Plan was concise and more readable.

Mr. Egner and **Ms. Alligood** suggested that the proposed amendments be available to the public in its entirety on Friday December 12th when the public notice was done but for the public hearings be broken up into focus sections. The Commission agreed.

Commissioner Lowcock thanked Ms. Alligood and Mr. Egner for their work on this project.

7.0 Planning Department Other Business/Updates

7.1 Planning Commission Notebook Update Pages

8.0 Planning Commission Discussion Items

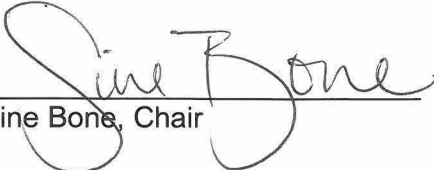
9.0 Forecast for Future Meetings:

- | | |
|------------------|---|
| January 13, 2015 | 1. Public Hearing: CPA-14-02 Moving Forward Milwaukie
Downtown Plan and Code Amendments #1 |
| | 2. Public Hearing: DR-14-07 Reliable Credit Parking Lot <i>continued
tentative</i> |
| January 27, 2015 | 1. Public Hearing: CPA-14-02 Moving Forward Milwaukie
Downtown Plan and Code Amendments #2 |

Meeting adjourned at approximately 9:33 p.m.

Respectfully submitted,

Alicia Martin, Administrative Specialist II


Sine Bone, Chair



AGENDA

MILWAUKIE PLANNING COMMISSION Tuesday, December 9, 2014, 6:30 PM

**MILWAUKIE CITY HALL
10722 SE MAIN STREET**

- 1.0 Call to Order - Procedural Matters**
- 2.0 Planning Commission Minutes** – Motion Needed
- 3.0 Information Items**
- 4.0 Audience Participation** – This is an opportunity for the public to comment on any item not on the agenda
- 5.0 Public Hearings** – Public hearings will follow the procedure listed on reverse
 - 5.1 Summary: Reliable Credit Parking Lot
Applicant/Owner: Sisul Engineering/L & B Holzman LLC
Address: 10605 SE Main St
File: DR-14-07
Staff: Vera Kolias
- 6.0 Worksession Items**
 - 6.1 Summary: CPA-14-02 Moving Forward Milwaukie Downtown Plan and Code Amendments – Downtown & Riverfront Land Use Framework Plan
Staff: Li Alligood and Denny Egner
- 7.0 Planning Department Other Business/Updates**
 - 7.1 Planning Commission Notebook Update Pages
- 8.0 Planning Commission Discussion Items** – This is an opportunity for comment or discussion for items not on the agenda.
- 9.0 Forecast for Future Meetings:**
 - January 13, 2015
 - 1. Public Hearing: CPA-14-02 Moving Forward Milwaukie Downtown Plan and Code Amendments #1
 - 2. Public Hearing: DR-14-07 Reliable Credit Parking Lot *continued tentative*
 - January 27, 2015
 - 1. Public Hearing: CPA-14-02 Moving Forward Milwaukie Downtown Plan and Code Amendments #2

Milwaukie Planning Commission Statement

The Planning Commission serves as an advisory body to, and a resource for, the City Council in land use matters. In this capacity, the mission of the Planning Commission is to articulate the Community's values and commitment to socially and environmentally responsible uses of its resources as reflected in the Comprehensive Plan

1. **PROCEDURAL MATTERS.** If you wish to speak at this meeting, please fill out a yellow card and give to planning staff. Please turn off all personal communication devices during meeting. For background information on agenda items, call the Planning Department at 503-786-7600 or email planning@ci.milwaukie.or.us. Thank You.
2. **PLANNING COMMISSION MINUTES.** Approved PC Minutes can be found on the City website at www.cityofmilwaukie.org
3. **CITY COUNCIL MINUTES** City Council Minutes can be found on the City website at www.cityofmilwaukie.org
4. **FORECAST FOR FUTURE MEETING.** These items are tentatively scheduled, but may be rescheduled prior to the meeting date. Please contact staff with any questions you may have.
5. **TIME LIMIT POLICY.** The Commission intends to end each meeting by 10:00pm. The Planning Commission will pause discussion of agenda items at 9:45pm to discuss whether to continue the agenda item to a future date or finish the agenda item.

Public Hearing Procedure

Those who wish to testify should come to the front podium, state his or her name and address for the record, and remain at the podium until the Chairperson has asked if there are any questions from the Commissioners.

1. **STAFF REPORT.** Each hearing starts with a brief review of the staff report by staff. The report lists the criteria for the land use action being considered, as well as a recommended decision with reasons for that recommendation.
2. **CORRESPONDENCE.** Staff will report any verbal or written correspondence that has been received since the Commission was presented with its meeting packet.
3. **APPLICANT'S PRESENTATION.**
4. **PUBLIC TESTIMONY IN SUPPORT.** Testimony from those in favor of the application.
5. **NEUTRAL PUBLIC TESTIMONY.** Comments or questions from interested persons who are neither in favor of nor opposed to the application.
6. **PUBLIC TESTIMONY IN OPPOSITION.** Testimony from those in opposition to the application.
7. **QUESTIONS FROM COMMISSIONERS.** The commission will have the opportunity to ask for clarification from staff, the applicant, or those who have already testified.
8. **REBUTTAL TESTIMONY FROM APPLICANT.** After all public testimony, the commission will take rebuttal testimony from the applicant.
9. **CLOSING OF PUBLIC HEARING.** The Chairperson will close the public portion of the hearing. The Commission will then enter into deliberation. From this point in the hearing the Commission will not receive any additional testimony from the audience, but may ask questions of anyone who has testified.
10. **COMMISSION DISCUSSION AND ACTION.** It is the Commission's intention to make a decision this evening on each issue on the agenda. Planning Commission decisions may be appealed to the City Council. If you wish to appeal a decision, please contact the Planning Department for information on the procedures and fees involved.
11. **MEETING CONTINUANCE.** Prior to the close of the first public hearing, *any person* may request an opportunity to present additional information at another time. If there is such a request, the Planning Commission will either continue the public hearing to a date certain, or leave the record open for at least seven days for additional written evidence, argument, or testimony. The Planning Commission may ask the applicant to consider granting an extension of the 120-day time period for making a decision if a delay in making a decision could impact the ability of the City to take final action on the application, including resolution of all local appeals.

The City of Milwaukie will make reasonable accommodation for people with disabilities. Please notify us no less than five (5) business days prior to the meeting.

Milwaukie Planning Commission:

Sine Bone, Chair
Wilda Parks, Vice Chair
Shannah Anderson
Scott Barbur
Greg Hemer
Shaun Lowcock
Gabe Storm

Planning Department Staff:

Denny Egner, Planning Director
Li Alligood, Senior Planner
Brett Kelter, Associate Planner
Vera Kolias, Associate Planner
Alicia Martin, Administrative Specialist II



To: Planning Commission
Through: Dennis Egner, Planning Director
From: Vera Kolias, Associate Planner
Date: December 2, 2014, for December 9, 2014, Public Hearing
Subject: File(s): DR-14-07
 Applicant: L&B Holzman, LLC (represented by Tom Sisul, Sisul Engineering)
 Address: 10605 SE Main St.
 Legal Description (Map & Taxlot): 1S1E35AA 00300 and 00800
 NDA: Historic Milwaukie

ACTION REQUESTED

Approve application DR-14-07 and the Findings and Conditions of Approval found in Attachments 1 and 2. This action would allow the construction of a new employee parking lot, including installation of parking lot lighting that is consistent with the style noted in the Downtown Design Guidelines and an edge treatment that is consistent with the design guidelines.

BACKGROUND INFORMATION

The applicant purchased the buildings at 1915 SE Harrison St and 10605 SE Main St in 1997. The applicant improved the office building and existing off-street parking area in 1998 through land use master file MU-98-01, and now intends to demolish an adjacent commercial building and is seeking Downtown Design Review approval for an employee parking lot with a net of 13 new parking spaces and associated landscaping, lighting, and stormwater facilities.

A. Site and Vicinity

The project site is located at 10605 SE Main Street, at the southwest corner of SE Main St and SE Scott St, and includes a small portion of the adjacent site located at 10633-10635 SE Main St. The project site is approximately 5,500 square feet and contains a commercial building approximately 4,200 square feet in area. The building storefronts face SE Main Street.

The surrounding area consists of both commercial and residential uses. A mixed use project that includes multifamily apartments (North Main) is across SE Main Street and restaurant, office and retail uses surround the property (see Figure 1).

B. Zoning Designation

The site is zoned Downtown Commercial (DC) (see Figure 2).

C. Comprehensive Plan Designation

Town Center (TC)

D. Land Use History

December 1998: Land use master file #MU-98-01, approved with conditions: The project approved improvements to the office building (making two buildings into one) and granted a variance for off-street parking. The project included applications for variances (from requirement to provide housing and to allow 19 parking spaces rather than 33 spaces). The project also included McLoughlin Corridor Overlay Review and transportation plan review.

E. Proposal

The applicant is currently seeking Downtown Design Review approval for construction of a parking lot.

The proposal includes the following (see Attachment 3.C, Plan Sheets):

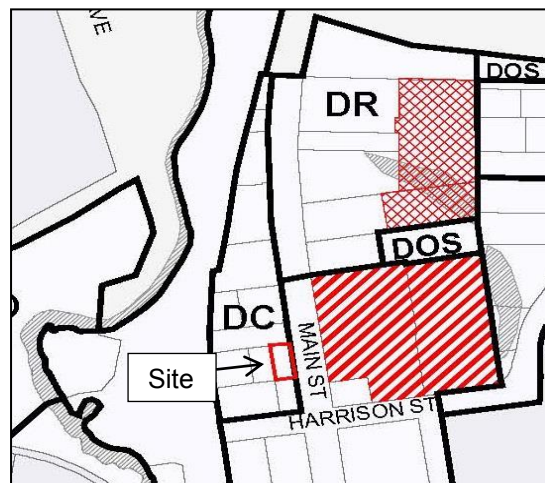
1. Construct a new 13-space employee parking area adjacent to the existing Reliable Credit building and site.
2. Provide interior and perimeter landscaping in the new parking area
3. Add lighting to match existing lighting

The Planning Commission is charged with reviewing the Downtown Design Review application (DR-14-07) for the parking lot. The Commission is not being asked for a decision on the demolition of the existing building. The demolition permit is an administrative permit that will be issued by the Building Official and is not subject to any further review by the City.

Figure 1. Site and vicinity



Figure 2. Zoning designations



F. DLC Meeting Summary

On December 1, 2014, the DLC held a public meeting to review the application. Based on the recommendations in the staff report, the applicant presented a revised design proposal that included ornamental light fixtures and a low seat wall along the site's SE Main Street frontage and the corner with SE Scott Street (See Attachment 3 (f)). After review and discussion, the DLC recommended approval of the application with conditions, which have been incorporated into the Recommended Findings and Conditions of Approval.

KEY ISSUES

Summary

Applicable Downtown Design Guidelines (See Attachment 1). Only relevant guidelines are listed.

1. Milwaukie Character Guidelines
 - Reinforce Milwaukie's Sense of Place
 - Strengthen the qualities and characteristics that make Milwaukie a unique place.
2. Pedestrian Emphasis Guidelines
 - Reinforce and Enhance the Pedestrian System
 - Barriers to pedestrian movement and visual and other nuisances should be avoided or eliminated, so that the pedestrian is the priority in all development projects.
 - Define the Pedestrian Environment
 - Provide human scale to the pedestrian environment, with variety and visual richness that enhance the public realm.
 - Provide Places for Stopping and Viewing
 - Provide safe, comfortable places where people can stop to sit and rest, meet and visit with each other, and otherwise enjoy the downtown surroundings.
3. Lighting Guidelines
 - Parking Lot Lighting
 - Ornamental street lights should be used to be compatible with downtown streetlight standards identified in the Public Area Requirements.

Staff has identified the following key issues for the Planning Commission's deliberation. Aspects of the proposal not listed below are addressed in the Findings (see Attachment 1) and generally require less analysis and discretion by the Commission.

- A. Does the proposed design sufficiently reinforce Milwaukie's sense of place and define and provide human scale to the pedestrian environment, as well as provide an area for stopping and viewing?
- B. Should new parking lot lighting be allowed to be consistent with existing parking lot lighting instead of with the ornamental style recommended by the Downtown Design Guidelines?

Analysis

A. Does the proposed design sufficiently reinforce Milwaukie's sense of place and define and provide human scale to the pedestrian environment, as well as provide an area for stopping and viewing?

The proposal is a surface parking lot, rather than a building. The challenge is to create a visually interesting environment for pedestrians on the public sidewalk without the benefit of a structure. The intent of the Pedestrian Emphasis guideline is to have a downtown that must "...maintain a clear and comfortable separation between pedestrian and vehicle areas..." and to "...provide human scale to the pedestrian environment, with variety and visual richness that enhance the public realm." The proposal includes landscaping of varying width, including red maple trees and water quality swale plantings of rushes, sedges, and several small native shrubs. This will create a planted edge that separates the pedestrian from parked vehicles. But it does not provide a very visually interesting space and does not create a distinctive edge between the public and private space.

In order to create a sense of enclosure and define the edge between the sidewalk and the parking lot, as well as provide an area for stopping and viewing, alternative solutions include an architectural/structural wall that mimics a storefront to maintain an uninterrupted street edge, or a low stone or brick seat wall. These would provide screening and enclosure and separate pedestrians from the vehicles and maintain a public space (the sidewalk) distinct from the private space (the parking lot). See Figures 3, 4 and 5 for examples in the Portland area.

DLC Review: On December 1, 2014, the DLC recommended that the parking lot design include a structural wall that mimics a storefront. The Committee's recommendation called for the applicant to submit at least two options for further review by the DLC.

The DLC found that the proposed landscaping solution is inconsistent with the objectives of the Downtown Design Guidelines in defining the pedestrian environment. The DLC recommended that the Commission require an architectural/structural wall that mimics a storefront to maintain a continuous street edge and that the wall include areas for seating. (see Attachment 1, Findings).

Figure 3. Seat wall



Figure 4. Architectural wall that mimics storefronts (without landscaping)



Figure 5. Architectural wall that mimics storefronts (with landscaping)



B. Should new parking lot lighting be allowed to be consistent with existing lighting instead of with the ornamental style recommended by the Downtown Design Guidelines?

The existing parking area was developed in 1998, prior to the adoption of the Downtown Design Guidelines in 2003. The applicant originally proposed to install new parking lot lighting in the same utilitarian style as the existing parking lot features. The existing lighting fixtures (see Figure 6) do not match the ornamental style prescribed in the Downtown Design Guidelines (see Figure 7) and are "not recommended." The applicant's revised submission, provided at the DLC meeting, included four ornamental light fixtures consistent with the downtown design guidelines.

The site is physically connected to the central downtown area, where other ornamental lights have been installed; specifically, across Main St from the site and further north as well. Maintaining consistency, given there are no immediate plans to install light fixtures in this block is important. Although other downtown parking areas do not have this ornamental lighting, those lots were developed prior to the adoption of the Downtown Design Guidelines. The applicant's original proposed site plan identifies two utilitarian light fixtures on the west side of the parking lot. Ornamental lighting that is consistent with the Downtown Design Guidelines, on the street side of the parking lot rather than the west side, would be more attractive and would provide lighting for both employees using the parking lot and pedestrians on the adjacent sidewalk. Further, the Downtown Design Guidelines recommend that lighting should be located in landscaped areas wherever possible, and no lighting currently exists on that side of Main Street. The fixture location should be coordinated with the planned location of any future light fixtures installed if/when Public Area Requirements are built. Per MMC 19.606.3.F, the luminaries should have shielding for limiting light trespass.

Figure 6. Existing parking lot light



Figure 7. Ornamental light



DLC Review: The DLC recommended during their review and discussion at the December 1, 2014 public meeting that a combination of light fixtures, both ornamental and utilitarian were appropriate for the site. In order to provide consistency and cohesiveness, the DLC recommended that ornamental fixtures be located adjacent to the public sidewalk. To provide additional lighting for safety, the DLC recommended that utilitarian fixtures (shoebox style) could be installed in the interior of the parking lot on the west side. The DLC recommends that two ornamental parking lot light fixtures shall also be installed on the east side of the parking lot, within the planting area adjacent to the public sidewalk. These revisions would be consistent with the Downtown Design Guidelines.

Other Code Requirements

1. MMC Section 19.609 Bicycle Parking

MMC 19.609 establishes standards for bicycle parking, including a requirement to provide a number of bicycle spaces equal to at least 10% of the minimum number of required vehicle spaces. If more than 10 bicycle spaces are required, then a minimum of 50% of the bicycle spaces must be covered and/or enclosed in lockers or a secure room.

The existing office use requires a minimum of 44 vehicle spaces; a minimum of 4 carpool spaces are required. According to the applicant's materials, the existing development provides 2 bicycle spaces. A condition has been developed to address this issue.

2. Per a review by the Engineering Department, a storm water management plan prepared by a qualified professional engineer with required development/building permits must be submitted as part of the proposed development. A condition has been developed to address this issue.

CONCLUSIONS

A. Staff recommendation to the Planning Commission is as follows:

1. Approve the Downtown Design Review application for the proposed parking lot construction, with conditions. This will result in the construction of a parking lot with revised light fixtures and location, and the provision of a structural edge treatment along the public sidewalk for the length of the parking lot.
2. Adopt the attached Findings and Conditions of Approval.

B. Staff recommends the following key conditions of approval (see Attachment 2 for the full list of Conditions of Approval):

- Two parking lot light fixtures shall be ornamental standards that are consistent with the downtown streetlight standards of the Public Works Standards. These ornamental parking lot light fixtures shall be located within the perimeter landscaping area along the Main Street sidewalk.
- Two additional parking lot light fixtures may be shoe-box style fixtures and shall be located along the western side in the interior of the parking lot.
- A structural edge treatment such as an architectural/structural wall that mimics a storefront shall be installed. The wall must be designed to accommodate the siting of the parking lot light fixtures and landscaping and shall include a seat wall. A minimum of two design alternatives showing specific materials with openings shall be presented to the DLC for review. The wall is intended to separate pedestrians on the public sidewalk from the parking lot, provide enclosure, and to define the pedestrian environment.
- The development permit submission for the parking lot shall include a total of 4 bicycle parking spaces for the Reliable Credit office use.

- Submit a storm water management plan prepared by a qualified professional engineer with required development/building permits as part of the proposed development. The plan shall conform to Section 2 – Stormwater Design Standards of the City of Milwaukie Public Works Standards.
 - The storm water management plan shall demonstrate that the post-development runoff does not exceed the pre-development, including any existing storm water management facilities serving the development site.
 - The storm water management plan shall demonstrate compliance with water quality standards in accordance with the City of Portland Stormwater Management Manual.

Development/building permits will not be issued for construction until the storm water management plan has been approved by the City of Milwaukie.

Other conditions of approval may be generated by the Planning Commission.

CODE AUTHORITY AND DECISION-MAKING PROCESS

The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC).

- MMC 19.1006 Type III Review
- MMC Section 19.907 Downtown Design Review
- MMC Chapter 19.700 Public Facility Improvements
- MMC Chapter 19.600 Off-Street Parking and Loading

This application is subject to Type III review, which requires the Planning Commission to consider whether the applicant has demonstrated compliance with the code sections shown above. In Type III reviews, the Commission assesses the application against review criteria and development standards and evaluates testimony and evidence received at the public hearing.

The Commission has four decision-making options as follows:

- A. Approve the application subject to the recommended Findings and Conditions of Approval.
- B. Approve the application with modified Findings and Conditions of Approval. Any modifications must be read into the record.
- C. Deny the application upon finding that it does not meet approval criteria.
- D. Continue the hearing.

The final decision on this application, which includes any appeals to the City Council, must be made by March 4, 2015, in accordance with the Oregon Revised Statutes and the Milwaukie Zoning Ordinance. The applicant can waive the time period in which the application must be decided.

COMMENTS

Notice of the proposed changes was given to the following agencies and persons: City of Milwaukie Community Development, Building, and Engineering Departments; Clackamas Fire District #1; Historic Milwaukie Neighborhood District Association (NDA); Clackamas County; Metro; and the Design and Landmarks Committee.

- **Mike Boumann, Deputy Fire Marshal, Clackamas Fire District #1:** No comment.
- **Samantha Vandagriff, Milwaukie Building Department:** No comments on the proposal at this time.
- **Brad Albert, Milwaukie Engineering Department:** Comments related to MMC Chapter 19.700 Public Facility Improvements, which is not applicable to the proposed development. Those comments have been incorporated into these findings. Other requirements related to stormwater management have been noted with the conditions of approval.
- **Dion Shepard, Chair, Historic Milwaukie NDA:** The Historic Milwaukie NDA opposes the project due to the fact that it is replacing Main Street businesses.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not subject to Commission review.

- **Ray Bryan, 11416 SE 27th Ave:** Opposed to the proposal due to the fact that it is removing Main Street businesses.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not subject to Commission review.

- **Wendy Wagner, 4015 SE Monroe St:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not subject to Commission review.

- **Reverend Melissa D. Meadows, 1550 SE Oak Grove Blvd Apt 208:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not subject to Commission review.

- **Mary Weaver, 11656 SE 48th Ave:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not subject to Commission review.

- **Barbara-Lee Orloff, 1400 SE Lava Drive:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not subject to Commission review.

- **Cassandra Caterson, 11363 SE 27th Ave:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not subject to Commission review.

ATTACHMENTS

Attachments are provided as indicated by the checked boxes. All material is available for viewing upon request.

	PC Packet	Public Copies	E- Packet
1. Recommended Findings in Support of Approval	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
2. Recommended Conditions of Approval	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
3. Applicant's Narrative and Supporting Documentation dated September 25, 2014 and October 29, 2014			
a. Narrative	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
b. Plan Sheets 1-5	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
c. Proposed lighting cut sheet	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
d. Photo of existing parking lot lighting	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
e. Preliminary storm drain planter calculations	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
f. DLC meeting exhibits (Dec. 1, 2014)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
4. Comments Received	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
5. Draft December 1, 2014 DLC Minutes	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

Key:

Early PC Mailing = paper materials provided to Planning Commission at the time of public notice 20 days prior to the hearing.

PC Packet = paper materials provided to PC 7 days prior to the hearing.

Public Copies = paper copies of the packet available for review at City facilities and at the Planning Commission meeting.

E-Packet = packet materials available online at

**Recommended Findings in Support of Approval
File # DR-14-07
Reliable Credit Parking Lot, 10605 SE Main Street**

Sections of the Milwaukie Municipal Code (MMC) not addressed in these findings are found to be inapplicable to the decision on this application.

1. The applicant, L&B Holzman, LLC (Reliable Credit), has applied for approval to construct additional off-street parking on the lot at 10605 SE Main Street. The site is in the Downtown Commercial zone and the proposal requires Downtown Design Review. The land use application file number is DR-14-07.
2. The applicant proposes to demolish an existing building and construct a new, 13-space employee parking area in its place for use by employees of Reliable Credit site. The applicant proposes interior and perimeter landscaping to be installed in the new parking area, along with additional lighting to match existing parking lot lighting, and stormwater infrastructure. Access to the proposed parking area will be from SE Main Street and the egress will be onto SE Scott Street.
3. The proposal is subject to the Milwaukie Downtown Design Guidelines and the following MMC provisions:
 - MMC Section 19.907 Downtown Design Review
 - MMC Subsection 19.304.6 Downtown Design Standards
 - MMC Chapter 19.600 Off-Street Parking and Loading
 - MMC Chapter 19.700 Public Facility Improvements
 - MMC Section 19.1000 Review Procedures
4. The application has been processed and public notice provided in accordance with MMC Section 19.1006 Type III Review and MMC Section 19.1011 Design Review Meetings. A public design review meeting was held before the DLC on December 1, 2014, and a public hearing was held before the Planning Commission on December 9, 2014, as required by law.
5. On December 1, 2014, the DLC held a public meeting to review the application. At this meeting the applicant presented a revised design proposal that included four ornamental light fixtures in the parking lot and a low seat wall along the site's SE Main Street frontage and the corner with SE Scott Street (See Attachment 3 (f)).
6. MMC Chapter 19.600 Off-Street Parking and Loading

MMC 19.600 regulates off-street parking and loading areas on private property outside the public right-of-way. The purpose of these requirements includes providing adequate space for off-street parking, minimizing parking impacts to adjacent properties, and minimizing environmental impacts of parking areas.

 - a. MMC Section 19.602 Applicability

MMC 19.602 establishes the applicability of the provisions of MMC 19.600.

(1) MMC Subsection 19.602.1 General Applicability

MMC 19.602.1 provides that the regulations of MMC 19.600 apply to all off-street parking areas, whether required by the City as part of development or voluntarily installed for the convenience of users. Activity that is not described by MMC Subsections 19.602.3 or 19.602.4 is exempt from compliance with the provisions of MMC 19.600.

The subject property includes existing off-street parking areas. The proposed development is an activity that meets the applicability standards of MMC 19.602.4 (see Finding 6-a-(3)).

(2) MMC Subsection 19.602.2 Maintenance Applicability

MMC 19.602.2 provides that property owners shall ensure conformance with the standards of MMC 19.600 with regard to ongoing maintenance, operations, and use of off-street parking areas. Any change to an existing off-street parking area shall not bring the area out of conformance, or further out of conformance if already nonconforming.

The proposed development will expand an existing off-street parking area. The proposed improvements are in conformance with the applicable standards of MMC 19.606, including for stall dimensions, landscaping, and lighting.

(3) MMC Subsection 19.602.4 Applicability not Associated With Development or Change in Use

MMC 19.602.4.A addresses applicability for parking projects developed to serve an existing use but not associated with other development activity or a change in use. Such activity shall conform to the requirements of MMC Sections 19.604 and 19.606-19.611. In addition, the total number of new spaces in the existing and new parking areas shall not exceed the maximum allowed quantity of parking as established in MMC Section 19.605.

The proposed development will serve an existing use on an adjacent property and is not associated with other development activity or a change in use. As addressed in Findings 5-c and 5-e through 5-g, the proposed development will meet all applicable standards of MMC Sections 19.604 and 19.606-611.

The Planning Commission finds that the standards and requirements of MMC 19.600 are applicable to the proposed development.

b. MMC Section 19.603 Review Process and Submittal Requirements

(1) MMC Subsection 19.603.1 Review Process

MMC 19.603.1 establishes the Planning Director as the entity with authority to apply the provisions of Chapter 19.600 unless an application is subject to a quasi-judicial review or appeal, in which case the body reviewing the application has the authority.

The application is subject to Type III review by the Planning Commission, which is therefore the body with authority to apply the provisions of Chapter 19.600.

(2) MMC Subsection 19.603.2 Submittal Requirements

MMC 19.603.2 establishes the requirements for submittal of a parking plan, including the various details that must be presented.

The applicant has submitted a parking plan and supporting information with sufficient detail to demonstrate compliance with the applicable standards of Chapter 19.600.

The Planning Commission finds that this standard is met.

c. MMC Section 19.604 General Parking Standards

MMC 19.604 establishes general standards for off-street parking areas, including requirements related to the provision of parking in conjunction with development activity, the location of accessory parking, and use and availability of parking areas.

The proposed development is an expansion of an existing off-street parking area related to an ongoing office use on an adjacent property. The new parking will be on an adjacent site owned by the same owner as the primary office use and will be available for the employees of the office use.

The Planning Commission finds that this standard is met.

d. MMC Section 19.605 Vehicle Parking Requirements

MMC 19.605 establishes standards to ensure that development provides adequate vehicle parking based on estimated parking demand.

(1) MMC Subsection 19.605.1 Minimum and Maximum Requirements

MMC Table 19.605.1 provides minimum and maximum requirements for a range of different uses. For general office uses, a minimum of 2 spaces per 1,000 sq ft of floor area are required; a maximum of 3.4 spaces per 1,000 sq ft are allowed.

The adjacent property is developed with an office building approximately 18,750 SF of floor area used for commercial purposes. A minimum of 44 parking spaces are required for that size of office use; a maximum of 74 spaces are allowed. The site includes a total of 21 spaces which falls below the minimum requirement for the current use.

The proposed development would result in a total of 34 spaces.

The Planning Commission finds that this standard is met.

e. MMC Section 19.606 Parking Area Design and Landscaping

(1) MMC Subsection 19.606.1 Parking Space and Aisle Dimension

MMC 19.606.1 establishes dimensional standards for required off-street parking spaces and drive aisles. For diagonal (45-degree-angle) spaces, the minimum width is 9 ft and minimum depth is 18.5 ft, with 13-ft-wide one-way drive aisles.

The applicant has submitted a parking plan that utilizes diagonal spaces. As proposed, the dimensions for new or reconfigured spaces and drive aisles appear to meet the minimum standards.

This standard is met.

(2) MMC Subsection 19.606.2 Landscaping

MMC 19.606.2 establishes standards for parking lot landscaping, including for perimeter and interior areas. The purpose of these landscaping standards is to provide buffering between parking areas and adjacent properties, break up large expanses of paved area, help delineate between parking spaces and drive

aisles, and provide environmental benefits such as stormwater management, carbon dioxide absorption, and a reduction of the urban heat island effect.

(a) MMC 19.606.2.C Perimeter Landscaping

In downtown zones, perimeter landscaping areas are not required along lot lines abutting other properties but must be at least 4 ft wide when abutting a right-of-way. At least 1 tree must be planted for every 40 lineal feet of landscaped buffer area, with the remainder of the buffer planted with grass, shrubs, ground cover, mulch, or other landscaped treatment.

As proposed, perimeter landscaping areas near the public right-of-way along SE Main Street are 4 feet wide and will have at least 1 tree planted every 40 lineal feet.

This standard is met.

(b) MMC 19.606.2.D Interior Landscaping

At least 25 sq ft of interior landscaped area must be provided for each parking space. Planting areas must be at least 120 sq ft in area, at least 6 ft in width, and dispersed throughout the parking area. For landscape islands, at least 1 tree shall be planted per island, with the remainder of the buffer planted with grass, shrubs, ground cover, mulch, or other landscaped treatment.

As proposed, interior landscaping areas appear to provide at least 750 sq ft of area, which is nearly twice the area required. New interior landscaping areas appear to be at least 6 ft wide and at least 120 sq ft in area, with at least 1 tree planted per island.

This standard is met.

(c) MMC 19.606.2.E Other Parking and Landscaping Provisions

Preservation of existing trees in the off-street parking area is encouraged and may be credited toward the total number of trees required. Parking area landscaping must be installed prior to final inspection, unless a performance bond is posted with the City. Required landscaping areas may serve as stormwater management facilities, and pedestrian walkways are allowed within landscape buffers if the buffer is at least 2 ft wider than required by MMC 19.606.2.C and 19.606.2.D.

The applicant has submitted a parking plan that preserves an existing tree just to the south of the subject property. As required, parking area landscaping will be installed prior to final inspection, unless a bond is posted with the City.

This standard is met.

(3) MMC Subsection 19.606.3 Additional Design Standards

MMC 19.606.3 establishes various design standards, including requirements related to paving and striping, wheel stops, pedestrian access, internal circulation, and lighting.

(a) MMC 19.606.3.A Paving and Striping

Paving and striping are required for all required maneuvering and standing areas, with a durable and dust-free hard surface and striping to delineate spaces and directional markings for driveways and accessways.

As proposed, the parking lot will be paved and striped.

This standard is met.

(b) MMC 19.606.3.B Wheel Stops

Parking bumpers or wheel stops are required to prevent vehicles from encroaching onto public right-of-way, adjacent landscaped areas, or pedestrian walkways. Curbing or other physical barriers may be substituted for wheel stops if vehicles will not encroach into the minimum required width for landscape or pedestrian areas.

As proposed, wheel stops will be installed.

This standard is met.

(c) MMC 19.606.3.C Site Access and Drive Aisles

Accessways to parking areas shall be the minimum number necessary to provide access without inhibiting safe circulation on the street. Drive aisles shall meet the dimensional requirements of MMC 19.606.1.

The parking area has one access point: one entrance from SE Main Street and one exit onto SE Scott Street. As proposed, all new drive aisles, as well as drive aisles modified by the proposed development, appear to meet the minimum dimensional requirements.

This standard is met.

(d) MMC 19.606.3.D Pedestrian Access and Circulation

Pedestrian access shall be provided so that no off-street parking space is further than 100 ft away, measured along vehicle drive aisles, from a building entrance or a walkway that is continuous, leads to a building entrance, and meets the design standards of Subsection 19.504.9.E.

As proposed, none of the new or modified parking spaces will be further than 100 ft from a pedestrian walkway. All new and modified pedestrian walkways will be reviewed through the development permitting process to ensure compliance with the various applicable standards established in Subsections 19.606.3.D and 19.504.9.E.

This standard is met.

(e) MMC 19.606.3.E Internal Circulation

The Planning Director has the authority to review the pedestrian, bicycle, and vehicular circulation of the site and impose conditions to ensure safe and efficient on-site circulation. Such conditions may include, but are not

limited to, on-site signage, pavement markings, addition or modification of curbs, and modification of drive aisle dimensions.

The Planning Director has reviewed the proposed parking plan and determined that no additional requirements are necessary to ensure safe and efficient on-site circulation.

This standard is met.

(f) MMC 19.606.3.F Lighting

Lighting is required for parking areas with more than 10 spaces and must have a cutoff angle of 90 degrees or greater to ensure that lighting is directed toward the parking surface. Lighting shall not cause a light trespass of more than 0.5 footcandles measured vertically at the boundaries of the site, and shall provide a minimum illumination of 0.5 footcandles for pedestrian walkways in off-street parking areas. Where practicable, lights shall be placed so they do not shine directly into any Water Quality Resource (WQR) and/or Habitat Conservation Area (HCA) location.

The proposed development will result in a total of 13 new parking spaces. As discussed in Finding 7-d, two ornamental lights will be provided in the parking lot consistent with the downtown design guidelines and two additional utilitarian lights matching the existing parking lot lighting style will be provided along the western edge of the parking lot. All new lighting will be reviewed through the development permitting process to ensure compliance with the various applicable illumination standards established in Subsection 19.606.3.F.

This standard is met.

As conditioned, the applicable additional design standards of MMC 19.606.3 will be met.

As conditioned, the Planning Commission finds that the applicable design and landscaping standards of MMC 19.606 will be met.

f. MMC Section 19.609 Bicycle Parking

MMC 19.609 establishes standards for bicycle parking, including a requirement to provide a number of bicycle spaces equal to at least 10% of the minimum number of required vehicle spaces. If more than 10 bicycle spaces are required, then a minimum of 50% of the bicycle spaces must be covered and/or enclosed in lockers or a secure room.

The existing office use requires a minimum of 44 vehicle spaces; a minimum of 4 bicycle spaces are required. According to the applicant's materials, the existing development provides 2 bicycle spaces. A condition has been developed to require 2 additional spaces in order to address this issue.

As conditioned, the Planning Commission finds that this standard is met.

g. MMC Section 19.610 Carpool and Vanpool Parking

MMC 19.610 establishes parking standards for vehicles used to carpool. For new development that requires at least 20 parking spaces, a minimum of 10% of the minimum amount of required parking shall be dedicated to carpool or vanpool

vehicles. Carpool spaces shall be located closer to the main entrances of the building than other employee parking and shall be clearly designated with signage or pavement markings.

The existing office use requires a minimum of 44 vehicle spaces; a minimum of 4 carpool spaces are required. The existing development provides 4 carpool spaces.

The Planning Commission finds that this standard is met.

7. MMC Section 19.907 Downtown Design Review

MMC 19.907 establishes the procedures and standards for design review in the downtown zones. The purpose of downtown design review is to preserve and enhance the character of downtown Milwaukie; to ensure a degree of order, harmony, and quality in the downtown zones; and to ensure that new development and enlargement of existing development are consistent with the Downtown Design Guidelines.

a. MMC Subsection 19.907.2 Applicability

MMC 19.907.2 establishes that all new construction and changes to buildings and/or properties in the downtown zones is subject to design review.

The proposed development involves the removal of a building and construction of a new parking lot, which is a change to the subject property.

The Planning Commission finds that the proposed development is subject to design review in accordance with the procedures provided in MMC Subsection 19.907.5.

b. MMC Subsection 19.907.5 Application Procedure

MMC 19.907.5 establishes the procedures by which applications for design review shall be processed. As per MMC 19.907.5.C, major exterior alterations are subject to Type III review. The Planning Director has determined that new construction is similar to a major exterior alteration with respect to the necessary level of review.

The Planning Commission finds that, as new construction, the proposed development is subject to Type III downtown design review.

c. MMC Subsection 19.907.6 Application

MMC 19.907.6 establishes the requirements for downtown design review applications, including a completed design review checklist, written statement describing how the proposal meets applicable design guidelines, and site plan showing the proposed development.

The applicant's submittal includes a completed design review checklist, narrative describing the proposed development and addressing applicable criteria, and site plans showing the proposed improvements.

The Planning Commission finds that this standard is met.

d. MMC Subsection 19.907.7 Approval Criteria for Design Review

MMC 19.907.7 establishes the approval criteria for design review applications and the process for modifications to the downtown design standards. The approval authority may approve, approve with conditions, or deny a design review application based on the following criteria:

Compliance with Title 19 Zoning Ordinance

The proposed development does not involve construction of a building. No standards from MMC 19.304 Downtown Zones are applicable to the project.

This approval criterion is not applicable.

Substantial consistency with the Downtown Design Guidelines

Refer to Table 1 below for detailed findings.

The Planning Commission finds that the proposal, as conditioned, is substantially consistent with the Downtown Design Guidelines and that this approval criterion has been met.

Submittal of a complete application and applicable fee as adopted by the City Council

The applicant submitted an application on September 25, 2014. It was deemed incomplete on October 9, 2014. The applicant submitted additional information on October 29, 2014 and the application was deemed complete on November 4, 2014. The applicable design review application fee was paid September 25, 2014.

This approval criterion has been met.

The Planning Commission finds that with the listed conditions the approval criteria for downtown design review are met.

Table 1. Design Review Compliance

MILWAUKIE CHARACTER GUIDELINES	
Guideline	Recommended Findings
<p><i>Reinforce Milwaukie's Sense of Place</i></p> <p><i>Strengthen the qualities and characteristics that make Milwaukie a unique place.</i></p>	<p>The design guidelines specifically state that development that is generic and could apply anywhere is not recommended. The proposed development is a new parking lot that, as proposed, does not provide any unique elements to reflect the character of Milwaukie's historic downtown. Although landscaping will be installed to provide a planted edge to both Scott Street and Main Street, a significant edge detail and treatment to emphasize a "small-town urban character" is needed and to maintain consistency with the character of the City's historic Main Street.</p> <p>The proposed development, as conditioned to provide a wall mimicking storefronts, meets this guideline.</p>
<p><i>Establish or Strengthen Gateways</i></p> <p><i>Projects should use arches, pylons, arbors or other transitions to mark special or primary entries and/or borders between public and private spaces.</i></p>	<p>The design guidelines address the transition between the public and private realm. Structural gateways achieve this by indicating change or separation of transportation modes, such as auto areas from pedestrian areas, such as the separation of the public sidewalk from the</p>

	<p>proposed parking lot.</p> <p>A structural wall along the east side of the parking lot, that would also the edge of the pedestrian environment, would be consistent with this guideline. The wall would provide screening and enclosure and separate pedestrians from the vehicles and maintain a public space (the sidewalk) distinct from the private space (the parking lot). A condition has been established to ensure that this guideline is met.</p> <p>The proposed development, as conditioned, meets this guideline.</p>
<i>(9 other guidelines related to Milwaukie Character)</i>	None of the other Milwaukie Character guidelines are applicable to this project.

PEDESTRIAN EMPHASIS GUIDELINES	
Guideline	Recommended Findings
<p><u>Reinforce and Enhance the Pedestrian System</u></p> <p><i>Barriers to pedestrian movement and visual and other nuisances should be avoided or eliminated, so that the pedestrian is the priority in all development projects.</i></p>	<p>The proposed development is within 100 feet of the employee entry and is adjacent to two public sidewalks.</p> <p>The proposed development meets this guideline.</p>
<p><u>Define the Pedestrian Environment</u></p> <p><i>Provide human scale to the pedestrian environment, with variety and visual richness that enhance the public realm.</i></p>	<p>The proposal includes landscaping of varying heights, including red maple trees and water quality swale plantings of rushes, sedges, and several small native shrubs. However, it does not provide a very visually interesting space and does not create a distinctive edge between the public and private space.</p> <p>In order to create a sense of enclosure and define the edge between the sidewalk and the parking lot, as well as provide an area for stopping and viewing, the guidelines can be satisfied by constructing a structural wall that includes a seating area. The wall would provide screening and enclosure and separate pedestrians from the vehicles and maintain a public space (the sidewalk) distinct from the private space (the parking lot).</p> <p>The proposed development, as conditioned, meets this guideline.</p>

<p><u>Provide Places for Stopping and Viewing</u></p> <p><i>Provide safe, comfortable places where people can stop to sit and rest, meet and visit with each other, and otherwise enjoy the downtown surroundings.</i></p>	<p>The proposal includes landscaping of varying width, including red maple trees and water quality swale plantings of rushes, sedges, and several small native shrubs. However, it does not provide places for the pedestrian to stop to site and rest, an important consideration on such a significant section of Main Street.</p> <p>In order to strengthen the edge between the sidewalk and the parking lot, as well as provide an area for stopping and viewing, the applicant shall include a structural wall that includes a seating area. A condition has been established to ensure that this guideline is met.</p> <p>The proposed development, as conditioned, meets this guideline.</p>
<p><i>(2 other guidelines related to Pedestrian Emphasis)</i></p>	<p>None of the other Pedestrian Emphasis guidelines are applicable to this project.</p>

ARCHITECTURE GUIDELINES

Guideline	Recommended Findings
<p><i>(12 guidelines related to Architecture)</i></p>	<p>The proposed development is the construction of a new parking lot and does not involve a building. The Architecture guidelines refer to buildings and are not applicable to this project.</p>

LIGHTING GUIDELINES

Guideline	Recommended Findings
<p><u>Parking Lot Lighting</u></p> <p><i>Ornamental street lights should be used to be compatible with downtown streetlight standards identified in the Public Area Requirements.</i></p>	<p>The existing parking area was developed in 1998, prior to the adoption of the Downtown Design Guidelines in 2003. The existing utilitarian lighting fixtures do not match the ornamental style prescribed in this guideline. The site is physically connected to the central downtown area, where other ornamental lights have been installed; specifically, across Main St from the site and further north as well.</p> <p>Maintaining consistency in light of no immediate plans to install light fixtures in this block is important. Although other downtown parking areas do not have this ornamental lighting, they were developed prior to the adoption of the Downtown Design Guidelines. The original site plan identifies two utilitarian light fixtures on the</p>

	<p>west side of the parking lot. The installation of ornamental lighting that is consistent with the Downtown Design Guidelines, on the street side of the parking lot rather than the west side, would be more attractive and would provide lighting for both employees using the parking lot and pedestrians on the adjacent sidewalk. Further, the Downtown Design Guidelines recommend that lighting should be located in landscaped areas wherever possible, and no lighting currently exists on that side of Main Street. The fixture location should align with the location of any future light fixtures installed if/when Public Area Requirements are built. Per MMC 19.606.3.F, the luminaries should have shielding for limiting light trespass.</p> <p>To be consistent with the guidelines, new ornamental lighting must be installed on the east side of the parking lot, within the planting area adjacent to the public sidewalk. These ornamental lights are to be in addition to the two utilitarian lights proposed along the western edge of the parking lot.</p> <p>As conditioned, the proposed development meets this guideline.</p>
<i>(3 other guidelines related to Lighting)</i>	None of the other Lighting guidelines are applicable to this project.

SIGN GUIDELINES	
Guideline	Recommended Findings
<i>(7 guidelines related to Signs)</i>	No signs are proposed as part of this application. The Sign guidelines are not applicable to this project.

8. MMC Chapter 19.700 Public Facility Improvements

MMC 19.700 establishes provisions to ensure that development provides public facilities that are safe, convenient, and adequate in rough proportion to their public facility impacts. MMC 19.702 establishes the applicability of the provisions of MMC 19.700, including land divisions, new construction, expansions of existing structures, and changes or intensifications in use.

The proposed development consists of the construction of a parking lot for an existing office use, which in and of itself does not increase the vehicle trip generation to and from

the site. The Engineering Department has reviewed the application and finds that MMC 19.700 does not apply to this application.

The Planning Commission finds that the standards and requirements of MMC 19.700 are not applicable to the proposed development.

9. The application was referred to the following departments and agencies on November 4, 2014:
 - Milwaukie Building Department
 - Milwaukie Engineering Department
 - Clackamas Fire District #1
 - Historic Milwaukie Neighborhood District Association Chairperson and Land Use Committee
 - Milwaukie Design and Landmarks Committee
 - Oregon Department of Transportation (ODOT)

Recommended Conditions of Approval
File # DR-14-07
Reliable Credit Parking Lot, 10605 SE Main Street

Conditions

1. The applicant shall submit a Type I Development Review application with final construction plans for construction of the parking lot. These plans shall be a modified version of the plans approved by the Planning Commission (PC) and date stamped by the City on September 25, 2014 and shall conform with these conditions of approval.
(Note: Any plan set changes proposed by the applicant, DLC, or Planning Commission during or as a result of the design review process shall be reflected in these conditions of approval as part of the adoption by the Planning Commission.)
2. The development permit submission for the parking lot shall include a detailed description of any proposed plan changes that are not part of these conditions of approval, or that the final decision-making authority did not specify in its decision; such plan change shall be subject to the City's review and approval.
3. The development permit submission for the parking lot shall include a total of 4 bicycle parking spaces for the office use at Reliable Credit.
4. The development permit submission for the parking lot shall include the following items to demonstrate conformance with the Milwaukie Downtown Design Guidelines, specifically those that address Milwaukie Character, the pedestrian environment, and parking lot lighting.
 - a. Two parking lot light fixtures shall be ornamental standards that are consistent with the downtown streetlight standards of the Public Works Standards. These ornamental parking lot light fixtures shall be located within the perimeter landscaping area along the Main Street sidewalk.
 - b. Two parking lot light fixtures may be shoe-box style fixtures and shall be located along the western side in the interior of the parking lot.
 - c. A structural edge treatment such as an architectural/structural wall that mimics a storefront shall be installed. The wall must be designed to accommodate the siting of the parking lot light fixtures and landscaping and shall include a seating area. A minimum of two design alternatives showing specific materials, with openings, and a seating area shall be presented to the DLC for review per Condition #5 below. The wall is intended to separate pedestrians on the sidewalk from the parking lot, provide enclosure, and to define the pedestrian environment.
5. The DLC shall review plans for the architectural/structural wall edge treatment to be incorporated into the design of the parking lot per Condition 4.c. above. The DLC shall, upon a finding by the majority of DLC members, confirm that the proposal is in substantial conformance with the Milwaukie Character and pedestrian environment guidelines. The applicant shall present the proposal at a public meeting that includes an opportunity for public comment.

Additional Requirements

1. Stormwater Management

Submit a storm water management plan prepared by a qualified professional engineer with required development/building permits as part of the proposed development. The plan shall conform to Section 2 – Stormwater Design Standards of the City of Milwaukie Public Works Standards.

- a. The storm water management plan shall demonstrate that the post-development runoff does not exceed the pre-development, including any existing storm water management facilities serving the development site.
- b. The storm water management plan shall demonstrate compliance with water quality standards in accordance with the City of Portland Stormwater Management Manual.

Development/building permits will not be issued for construction until the storm water management plan has been approved by the City of Milwaukie.

2. Pursuant to Subsection 19.1001.7.E.2, the time period within which the applicant must obtain development permits for the parking lot is 2 years, and the time period within which the applicant must pass all final inspections is 4 years, from the date of the land use decision on this application.

As per MMC 19.1001.7.E.2.b, land use approvals shall expire unless both steps noted above have been completed or unless the review authority specifies a different expiration date in the land use decision to accommodate large, complex, or phased development projects.



PLANNING DEPARTMENT
6101 SE Johnson Creek Blvd
Milwaukie OR 97206

PHONE: 503-786-7630
FAX: 503-774-8236
E-MAIL: planning@milwaukieoregon.gov

Application for Land Use Action

Master File #: DP-14-07

Review type*: ☐ I ☐ II ☒ III ☐ IV ☐ V

CHOOSE APPLICATION TYPE(S):

Downtown Design Review

...

...

...

...

Use separate application forms for:

- Annexation and/or Boundary Change
- Compensation for Reduction in Property Value (Measure 37)
- Daily Display Sign
- Appeal

RESPONSIBLE PARTIES:

APPLICANT (owner or other eligible applicant—see reverse): L & B Holzman, LLC

Mailing address: PO Box 22829, Milwaukie, OR Zip: 97269

Phone(s): 503-462-3000 E-mail: lmholzman@reliablecredit.com

APPLICANT'S REPRESENTATIVE (if different than above): Tom Sisul, Sisul Engineering

Mailing address: 375 Portland Avenue, Gladstone, OR Zip: 97027

Phone(s): 503-657-0188 E-mail: tomsisul@sisulengineering.com

SITE INFORMATION:

Address: 10605 SE Main Street Map & Tax Lot(s): 11E35AA, 300, 600 & 700

Comprehensive Plan Designation: TC Zoning: DC Size of property: 15,400.00 Sq Ft

PROPOSAL (describe briefly):

To create new a parking area for Reliable Credit employees on TL 300, which is currently occupied by an older commercial building.

SIGNATURE:

ATTEST: I am the property owner or I am eligible to initiate this application per Milwaukie Municipal Code (MMC) Subsection 19.1001.6.A. If required, I have attached written authorization to submit this application. To the best of my knowledge, the information provided within this application package is complete and accurate.

Submitted by:

lm Holzman Manager

Date:

9/22/14

IMPORTANT INFORMATION ON REVERSE SIDE

Attach all required submittals.

WHO IS ELIGIBLE TO SUBMIT A LAND USE APPLICATION (excerpted from MMC Subsection 19.1001.6.A):

Type I, II, III, and IV applications may be initiated by the property owner or contract purchaser of the subject property, any person authorized in writing to represent the property owner or contract purchaser, and any agency that has statutory rights of eminent domain for projects they have the authority to construct.

Type V applications may be initiated by any individual.

PREAPPLICATION CONFERENCE:

A preapplication conference may be required or desirable prior to submitting this application. Please discuss with Planning staff.

REVIEW TYPES:

This application will be processed per the assigned review type, as described in the following sections of the Milwaukee Municipal Code:

- Type I: Section 19.1004
- Type II: Section 19.1005
- Type III: Section 19.1006
- Type IV: Section 19.1007
- Type V: Section 19.1008

THIS SECTION FOR OFFICE USE ONLY:

FILE TYPE	FILE NUMBER	FEE AMOUNT*	PERCENT DISCOUNT	DISCOUNT TYPE	DEPOSIT AMOUNT	DATE STAMP
Master file	DR-14-07	\$ 2000-			\$	RECEIVED SEP 25 2014 CITY OF MILWAUKIE PLANNING DEPARTMENT
Concurrent application files		\$			\$	
		\$			\$	
		\$			\$	
		\$			\$	
SUBTOTALS		\$			\$	
TOTAL AMOUNT RECEIVED: \$ 2000-		RECEIPT #: 561280		RCD BY: A. Martin		
Associated application file #s (appeals, modifications, previous approvals, etc.):						
Neighborhood District Association(s): <i>Historic Milwaukee</i>						
Notes:						

*After discount (if any)

SUBMIT

RESET



PLANNING DEPARTMENT
6101 SE Johnson Creek Blvd
Milwaukie OR 97206

PHONE: 503-786-7630
FAX: 503-774-8236
E-MAIL: planning@milwaukieoregon.gov

For all Land Use Applications
(except Annexations and Development Review)

Submittal Requirements

All land use applications must be accompanied by a signed copy of this form (see reverse for signature block) and the information listed below. The information submitted must be sufficiently detailed and specific to the proposal to allow for adequate review. Failure to submit this information may result in the application being deemed incomplete per the Milwaukie Municipal Code (MMC) and Oregon Revised Statutes.

Contact Milwaukie Planning staff at 503-786-7630 or planning@milwaukieoregon.gov for assistance with Milwaukie's land use application requirements.

1. All required land use application forms and fees, including any deposits.

Applications without the required application forms and fees will not be accepted.

2. Proof of ownership or eligibility to initiate application per MMC Subsection 19.1001.6.A.

Where written authorization is required, applications without written authorization will not be accepted.

3. Detailed and comprehensive description of all existing and proposed uses and structures, including a summary of all information contained in any site plans.

Depending upon the development being proposed, the description may need to include both a written and graphic component such as elevation drawings, 3-D models, photo simulations, etc. Where subjective aspects of the height and mass of the proposed development will be evaluated at a public hearing, temporary on-site "story pole" installations, and photographic representations thereof, may be required at the time of application submittal or prior to the public hearing.

4. Detailed statement that demonstrates how the proposal meets all applicable application-specific approval criteria (check with staff) and all applicable development standards (listed below):

- a. **Base zone standards** in Chapter 19.300.
- b. **Overlay zone standards** in Chapter 19.400.
- c. **Supplementary development regulations** in Chapter 19.500.
- d. **Off-street parking and loading standards and requirements** in Chapter 19.600.
- e. **Public facility standards and requirements**, including any required street improvements, in Chapter 19.700.

5. Site plan(s), preliminary plat, or final plat as appropriate.

See Site Plan, Preliminary Plat, and Final Plat Requirements for guidance.

6. Copy of valid preapplication conference report, when a conference was required.

APPLICATION PREPARATION REQUIREMENTS:

- Five copies of all application materials are required at the time of submittal. Staff will determine how many additional copies are required, if any, once the application has been reviewed for completeness.
- All application materials larger than 8½ x 11 in. must be folded and be able to fit into a 10- x 13-in. or 12- x 16-in. mailing envelope.
- All application materials must be collated, including large format plans or graphics.

ADDITIONAL INFORMATION:

- Neighborhood District Associations (NDAs) and their associated Land Use Committees (LUCs) are important parts of Milwaukie's land use process. The City will provide a review copy of your application to the LUC for the subject property. They may contact you or you may wish to contact them. Applicants are strongly encouraged to present their proposal to all applicable NDAs prior to the submittal of a land use application and, where presented, to submit minutes from all such meetings. NDA information: <http://www.milwaukieoregon.gov/communityservices/neighborhoods-program>.
- Submittal of a full or partial electronic copy of all application materials is strongly encouraged.

As the authorized applicant I, (print name) THOMAS J. SISK, attest that all required application materials have been submitted in accordance with City of Milwaukie requirements. I understand that any omission of required items or lack of sufficient detail may constitute grounds for a determination that the application is incomplete per MMC Subsection 19.1003.3 and Oregon Revised Statutes 227.178. I understand that review of the application may be delayed if it is deemed incomplete.

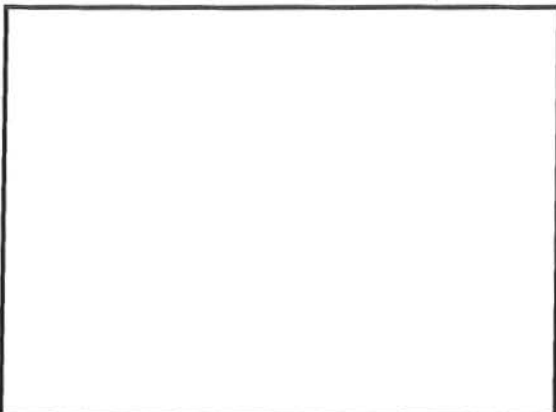
Furthermore, I understand that, if the application triggers the City's sign-posting requirements, I will be required to post signs on the site for a specified period of time. I also understand that I will be required to provide the City with an affidavit of posting prior to issuance of any decision on this application.

Applicant Signature: Thomas J. Sisk (FOR THE APPLICANT)

Date: SEPT 23, 2014

Official Use Only

Date Received (date stamp below):



DOWNTOWN DESIGN REVIEW CHECKLIST

Project/Applicant Name: RELIABLE CREDIT'S PARKING LOT EXPANSION
 Project Address: 10605 SE MAIN ST.
 Application Submission Date: SEPT. 2014
 Zoning: DOWNTOWN COMMERCIAL
 Building Use: BANK
 Completed By: TOM SISUL on: 9-25-2014

STANDARDS AND GUIDELINES

A. Development and Design Standards		Complies		
		Yes	No	NA
1. Development Standards				
a.	Permitted Use	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b.	Minimum Lot Size	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	Floor Area Ratio	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	Building Height	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	Residential Density	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f.	Street Setbacks	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
g.	Side and Rear Setbacks	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h.	Ground-floor Retail	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i.	Ground-floor Windows/Doors	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j.	Drive-through Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
k.	Off-street Parking Requirements	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
l.	Landscaping	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Design Standards				
a.	Residential Entries and Porches	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	Garages and Parking Areas	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	Courtyards	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	Balconies	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	Walls	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f.	Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g.	Roofs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
B. Design Guidelines				
1. Milwaukie Character				
a.	Reinforce Milwaukie's Sense of Place	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	Integrate the Environment	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	Promote Linkages to Horticultural Heritage	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	Establish or Strengthen Gateways	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	Consider View Opportunities	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f.	Consider Context	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g.	Promote Architectural Compatibility	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h.	Preserve Historic Buildings	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i.	Use Architectural Contrast Wisely	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j.	Integrate Art	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

DOWNTOWN DESIGN REVIEW CHECKLIST

		Complies		
		Yes	No	NA
2. Pedestrian Emphasis				
a.	Reinforce and Enhance the Pedestrian System.....	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	Define the Pedestrian Environment.....	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	Protect the Pedestrian from the Elements.....	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	Provide Places for Stopping and Viewing	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	Create Successful Outdoor Spaces	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f.	Integrate Barrier-Free Design	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Architecture				
a.	Corner Doors	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	Retail and Commercial Doors.....	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	Residential Doors	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	Wall Materials	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	Wall Structure	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f.	Retail Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g.	Residential Bay Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h.	Silhouette and Roofline	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i.	Rooftops	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j.	Green Architecture	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
k.	Building Security	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
l.	Parking Structures	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Lighting				
a.	Exterior Building Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	Parking Lot Lighting	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c.	Landscape Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	Sign Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. Signs				
a.	Wall Signs.....	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	Hanging or Projecting Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	Window Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	Awning Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	Information and Guide Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f.	Kiosk Monument Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g.	Temporary Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Notes:

Z:\Planning\Administrative - General Info\Handouts\DtnDesignRevCL(Applicant).doc—Last rev. 5/14/11

RECORDING REQUESTED BY:



\$52.00

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04/15/2010 11:09:27 AM

D-D Cnt=1 Stn=6 KARLYNWUN
\$10.00 \$10.00 \$16.00 \$16.00

Recorded by TICOR TITLE

GRANTOR'S NAME:

David S. Strauss and Naomi K. Strauss,
Co-Trustees of the David S. Strauss Trust dated
July 31, 1992

GRANTEE'S NAME:

L & B Holzman, L.L.C., an Oregon limited liability
company

SEND TAX STATEMENTS TO:

L & B Holzman, L.L.C., an Oregon limited liability
company
PO Box 22829

Milwaukie, OR 97269

AFTER RECORDING RETURN TO:

L & B Holzman, L.L.C., an Oregon limited liability
company
PO Box 22829
Milwaukie, OR 97269

Escrow No: 3626025175AMS-TTPOR50

CODE 012-002
00017966, 11E35AA-00300
10605 SE Main Street
Milwaukie, OR 97222

SPACE ABOVE THIS LINE FOR RECORDER'S USE

STATUTORY WARRANTY DEED

David S. Strauss and Naomi K. Strauss, Co-Trustees of the David S. Strauss Trust dated July 31, 1992,
Grantor, conveys and warrants to

L & B Holzman, L.L.C., an Oregon limited liability company, Grantee, the following described real property,
free and clear of encumbrances except as specifically set forth below, situated in the County of
Clackamas, State of Oregon:

Lot 8, Block 1, LEWELLING PARK, in the City of Milwaukie, County of Clackamas, State of
Oregon.

Subject to and excepting: None

BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON TRANSFERRING FEE TITLE
SHOULD INQUIRE ABOUT THE PERSON'S RIGHTS, IF ANY, UNDER ORS 195.300, 195.301 AND
195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, AND
SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009. THIS INSTRUMENT DOES NOT
ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF
APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS
INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH

**Application Narrative
for
Reliable Credit's Parking Lot Expansion
(Revised October 2014)**

Applicant	L & B Holzman, LLC PO Box 22829 Milwaukie, OR 97269
Owner	L & B Holzman, LLC
Location	10605 SE Main Street
Legal Description	Tax Lots 300, 600 and 700 Sec. 35AA, T1S R1E
Zoning	DC, Downtown Commercial
Lot Size Areas	TL 300 – 5,504 SF; TL 600 – 5,500 SF; TL 700 – 15,400 SF
Proposal	Construct Parking Lot Expansion on TL 300 and modify striping and ADA stall access on TL 700

PROPOSAL

To construct 13 stall parking lot expansion, mostly on TL 300. Modify some parking lot striping and ADA stall access on TL's 600 and 700. Additional parking lot capacity is to accommodate employee parking needs.

No building changes or additions are proposed as a part of the application other the removal of the existing building at 10605 SE Main Street.

SITE DESCRIPTION

The Reliable Credit site currently exists along Harrison Street between Highway 99E and Main Street. The additional parking area would occur on an adjacent parcel north of the existing Reliable Credit site adjacent to Main Street and includes frontage on Scott Street.

Tax Lots 600 and 700 will have only minor changes on them. TL 300 presently has a building that will be torn down to make room for the parking lot expansion. TL 300 is entirely covered by either the building or a driveway behind the building with access via Scott Street.

All parcels involved are very flat. There is presently no landscaping on TL 300. There are landscape areas on TL 600 and 700.

Public storm drainage is available in Scott Street via what appears may be a combined sewer and in Main Street in what appears is more likely a storm drain only system.

Applicable Criteria and Standards

The applicable requirements for the proposed parking lot expansion are listed and discussed in the following narrative:

Milwaukie Municipal Code

Chapter 19.907 Downtown Design Review

19.907.2 Applicability

All new construction ... as defined in Subsection 19.304.6.B are subject to design review in accordance with the procedures as outlined below under Subsection 19.907.5. *Per subsection 19.304.6.B this will qualify as subject to design review.*

19.907.3 Design Guidelines

Design guidelines shall be established for the downtown zones and shall be considered as part of the design review application in accordance with Section 19.304. *Applicable design guidelines and code sections are addressed below.*

19.907.5 Application Procedure

19.907.5.C Major Exterior Alterations – Mayor exterior alterations, as defined in Subsection 19.304.6.B.3 shall be evaluated through a Type III review in accordance with the procedures in Section 1006. *Per Section 19.304.6.B.3 this will be considered a major exterior alteration and thus will be processed as a Type III application.*

19.907.6 Application

A) Completed design review checklist. *A completed design review checklist is included with the application materials.*

B) Written statement that describes how the proposal meets applicable design guidelines. *A written narrative is included the application materials that address applicable design guidelines.*

C) Show footprints of surrounding buildings, including driveway and pedestrian connections. *Footprints of existing surrounding buildings are shown on application plans. No surrounding driveway or pedestrian connections exists on the adjoining parcels except for those on the site itself which are shown and the public street sidewalks.*

D) Location, dimension, and setbacks of all proposed buildings, structures, walls, and fences. *All such features are shown on the application plans.*

E) Dimensioned building elevations indicating height, exterior materials, colors and details of exterior architectural features. *This is not applicable the application as no building alterations are proposed other than the removal of an existing structure.*

F) A streetscape showing the relationship of the proposed project to adjacent buildings. *A streetscape plan is a part of application plans.*

G) Frontage improvements in the public right-of-way per the Public Area Requirements. *No frontage improvements are required other than the removal of the existing driveway approach and installation of a new driveway approach.*

19.907.8 Report and Recommendation by Design and Landmarks Committee - *The process will require a Type III process, therefore the Design and Landmarks Committee review is a part of the process.*

19.907.9 Variances to Development Standards – *No variances to the development standards under Section 304.4 is requested.*

Chapter 19.304, Downtown Zones

19.304.3 Uses - *A parking facility is a permitted use in the Downtown Commercial Zoning.*

19.304.4 Development Standards – *Off street parking is required in the Downtown Commercial Zoning. Landscaping minimum requirement is 10% for the Downtown Commercial Zoning.*

19.304.4.B.10 Off Street Parking

b) *As the site is neither in the Downtown Storefront Zone nor the Downtown Office Zone the off-street parking standards of 19.600 are applicable.*

19.304.4.B.11 Minimum Landscaping/Open Space

b) *Existing streets trees on SE Main will remain. A new street tree is proposed at the present driveway location that will be removed, along Scott Street. However, the existence of a nearby utility pole and overhead lines may present issues in installing a street tree at the proposed location.*

c) *All landscape areas are to be planted with live plants.*

19.304.6.B Applicability

3) Major exterior alterations include any of the following:

b) Demolition or replacement of more than 25% of the surface area of any wall or roof. *An entire building will be removed and thus this subsection of the code is applicable.*

Chapter 19.400 Overlay Zones

19.401 Willamette Greenway Zone – *The site lies outside the Willamette Greenway overlay and thus this section of the code is not applicable.*

19.402 Natural Resources – *The site lies outside any noted natural resource area and the 100 foot boundary surrounding such areas. This section of the code is not applicable.*

19.403 Historic Preservation Overlay – *The parcels involved are not on the Historic Preservation Property List. This code section is not applicable.*

19.404 Mixed Use Overlay Zone – *This site is not within a mixed use overlay zone and thus the code section is not applicable.*

19.405 Aircraft Landing Facility Zone – *Such a designation does not exist on this parcel and thus is not applicable to the site.*

19.406 Tacoma Station Area Overlay Zone – *The site does not lie within any the Tacoma Station Overlay Zones and thus those code sections are not applicable.*

Chapter 19.500 Supplementary Development Regulations

19.504-1 Clear Vision Areas – *This section is applicable as the site improvement does lie at the intersection of two streets. Clear vision areas at the corner in accordance with Chapter 12.24 will be met.*

19.504.7 Minimum Vegetation – *A landscape plan in accordance with the requirements of this section area a part of the application submittals.*

19.504.9 On-site Walkways and Circulation – *This code section does not appear to be applicable per Section 19.702.1. See Section 19.702 addressed below.*

Chapter 19.600 Off-street Parking and Loading Standards and Requirements

19.602.1 General Applicability – *Voluntarily installed parking facilities fall under Subsection 19.602.4*

19.602.4.A Parking areas developed to serve an existing use that is not associated with development activity or a change in use shall conform to the requirements of Sections 19.604 and 19.606-19.611. *Those code sections are address below. The total number of spaces in the existing parking area and new parking area shall not exceed the maximum allowed quantity of parking as established in Section 19.605 The total number of spaces in the existing and new parking area do not exceed maximum allowed quantity of parking as established in Section 19.605 and noted below.*

19.604.2 Parking Area Location – *Accessory parking for this development is per the requirements of 19.604.2.A as it lies on the same site (an adjoining tax lot) as the primary use.*

19.605.1 Minimum and Maximum Requirements

A) Development shall provide at least the minimum and not more than the maximum number of parking spaces as listed in Table 19.605.1 *Per Table 19.605.1, Category F – Commercial Uses-Services, Item #1 General Office, including banks, the minimum number of parking spaces per 1,000 SF of floor area shall be at least 2 and the maximum not more than 3.4. The Reliable Credit building contains approximately 18,750 SF of floor area used for commercial purposes and approximately 3750 SF is for storage only. As we do not see a different parking ratio for*

commercial storage area we are assuming the same minimum and maximums are applicable. Therefore, the minimum number of parking stalls required by code would be 44 and maximum would be 74. We note that even with the additional on-site parking proposed the number of parking stalls on the site will still not reach the minimum number of stalls that would normally be required by this code section. However, the deficiency will be significantly reduced.

19.606.1 Parking Space and Aisle Dimensions

A) *The proposed parking facility will meet the off-street parking space requirements and aisle requirements for 45° parking as noted on Table 19.606.1.*

B) *One additional ADA stall will be added near the main entrance to the Reliable Credit building. It will meet the requirements of federal and state requirements.*

C) *Proposed parking stalls will be able to be entered by vehicles in a forward manner.*

D) *A drive aisle is proposed and will meet the requirements of Table 19.606.1 for one directional travel to serve 45° parking.*

19.606.2 Landscaping

B) General Provisions

1) *Landscaping for this parking area is required.*

C) Perimeter Landscaping

1) Dimensions - Per Table 19.606.2.C.1 the minimum perimeter landscape strip adjacent to the right-of-way line is 4 feet in the Downtown Zones. *This standard will be met. The minimum landscape strip abutting an adjacent lot is 0', except for properties that share a parking area. There is not a shared parking area proposed.*

2) Planting Requirements - Landscape requirements for perimeter buffer areas shall include 1 tree planted per 40 lineal feet of landscape buffer area. *This standard will be met. The remainder of the buffer area shall be grass, ground cover, mulch, shrubs, trees or other landscape treatment other than concrete and pavement. The ground cover will be vegetated in accordance with a storm water quality facility where appropriate and with more standard vegetation in other areas.*

3) Additional Planting Requirements Adjacent to Residential Uses – *This is not applicable to this site as abutting uses are commercial.*

D) Interior Landscaping

1) General Requirements – Interior Landscaping shall be provided for site where there are more than 10 parking spaces on the entire site. *This is applicable to the subject site as there are more than 10 parking stalls.*

2) Required Amount of Interior Landscape Area – At least 25 square feet of landscape area must be provided for each parking area. Planting areas must be at least 120 square feet in area and dispersed throughout the parking area. *Fifteen spaces will require a minimum of 375 SF of interior landscape area and more than 750 SF of interior landscaping will be provided, all of which is connected to perimeter landscape areas.*

3) Required Amount of Interior Landscaped Area

a) Interior landscape area shall be either a divider median between opposing rows of parking or a landscape island in the middle or at the end of the row. *There is no opposed parking stalls in the proposed improvement. Interior landscaping areas will be met via the triangular landscape areas created by the diagonal parking along the edges and ends of the diagonal parking stalls.*

b) Interior landscape must be a minimum of 6 feet in width. Where a curb provides the border for an interior landscape area, the dimension shall be measured from inside of the curbs. *All interior landscape areas will average at least 6 feet in width. See plan for interior landscape dimensions.*

4) Planting requirements for Interior Landscape Areas

a) For divider medians at least 1 shade or canopy tree must be provided for every 40 linear feet. *No divider medians are proposed.*

b) For landscape islands at least 1 tree shall be planted per island. Along the street frontage the interior landscape areas are contiguous with the exterior landscape area which will meet the 40 foot spacing standard. *On the west side where the landscape area is a series of smaller connected triangular areas one tree per 40 linear feet will be planted. So that the entire perimeter of the parking area will be bordered by trees planted a spacing of 40 feet or less.*

c) The remainder of the landscape islands shall be grass, ground cover, mulch, shrubs, trees or other landscape treatment. *The interior landscape areas near Main Street will be a part of the stormwater quality facility and planted in accordance with such a facility. The other areas will be planted with shrubs similar to the existing landscape areas around the existing Reliable Credit parking area.*

5) Additional Landscaping for Large Parking Areas – *This section is not applicable as the number of parking stalls will be less than 100.*

E) Other Parking Area Landscape Provisions

1) Preservation of existing trees is encouraged. *At present the site does not have any landscape areas. Trees on the existing parking Reliable Credit where the access will be to enter this new parking area, are being retained to the extent possible, please see the plans, although 3 8 inch diameter trees will have to be removed.*

4) Required parking landscape areas may serve as stormwater management facilities for the site. *As noted above the intent is the landscape area adjacent to Main Street will be a part of the stormwater quality facilities.*

5) Pedestrian walkways are allowed within the perimeter and interior landscape buffer. *No walkways are proposed.*

19.606.3 Additional Design Standards

A) Paving and Striping – *Standard asphalt pavement will be used for surfacing, and striping will denote the diagonal stalls.*

B) Wheel Stops – *Wheel Stops will be used.*

C) Site Access and Drive Aisles

1) Accessways to parking areas shall be the minimum necessary. *A single accessway is proposed. Driveway approaches shall comply with the access spacing standards of Chapter 12.16. The standards of 12.16 will be met.*

2) Drive aisles shall meet the dimensional requirements in Subsection 19.606.1. *The required 13 foot driveway aisle requirement for 45° parking will be provided.*

3) Parking drive aisles shall align with the approved driveway access and shall not be wider than the approved driveway access within 10 feet of the right-of-way boundary. *This requirement will be met.*

4) Along collector and arterial street, no parking space shall be located such that its maneuvering area is in an ingress egress aisle within 20 feet of the back of the sidewalk. *While Main Street is neither a collector nor an arterial, the 20 foot requirement will be met regardless.*

5) Driveways and on-site circulation shall be designed so that the vehicles enter the right-of-way in a forward motion. *This requirement will be met.*

D) Pedestrian Access and Circulation *(There are also Downtown Design Guidelines with regards to Pedestrian Emphasis Guidelines which are also address below.)*

1) Pedestrian access shall be provided for off-street parking areas so that no parking space is more than 100 feet away, measured along drive aisles from a building entrance, or a walkway. *All proposed parking stalls will be either within the 100 feet of the employee entry (as the intent of these parking stalls is for employee parking) and/or the public sidewalks along Main or Scott Streets.*

2) Walkways through off-street parking areas must be continuous. *No walkways are proposed.*

Pedestrian Emphasis Guidelines from the Downtown Design Guidelines

Reinforce and Enhance the Pedestrian System – *The proposed parking area is intended only for employees of Reliable Credit and it does meet the Code requirements as noted above. The distance from existing public sidewalks to the nearest edge of furthest parking stall will be less than 60 feet, via walking on pavement. Considering that more than 75 feet separate the sidewalks on McLoughlin, at the Harrison intersection and approximately 50 feet between sidewalks on opposite sides of Harrison in front of the Reliable Credit building, the distance to the nearest sidewalk from any of the proposed parking stalls is reasonable.*

Define the Pedestrian Environment – *The proposed parking area is adjacent to City public sidewalk on two sides, although separated by required landscape strips. The landscape strips will be irregularly sized and not a continuous width, which will add some variety to the visual aspect to the pedestrians walking along the adjacent public sidewalks. In addition, portions of this landscape area will serve as water quality facility allowing pedestrians to see some urban green water quality treatment at close range.*

E) Internal Circulation

2) Connections to Adjacent Parking Areas – *There are no parking areas on adjacent sites.*

3) Drive-Through Uses and Queuing Areas – *No such facilities are proposed with this application.*

F) Lighting- Lighting is required for parking areas with more than 10 spaces. *As this site will have more than 10 spaces lighting in the proposed parking area will be provided meeting the standards of this section. The Milwaukie Downtown Design Guidelines for parking lot lighting recommends:*

- *Historical parking lot lights. The owners wish to have parking lot lighting similar to what presently exists on in their existing parking lot, which is a down facing "shoebox" style light fixture. This style of light is also more appropriate towards meeting the "Dark Skies Initiative" then most of the ornamental style light fixtures noted in the design guidelines.*
- *Pole standards should be black or a very dark green. The light poles will be black.*
- *Standards may accommodate banner or hanging flower pots. As light poles are intended to be located away from the public sidewalk, standards are not proposed to be included.*
- *Light standards should be located in landscape areas wherever possible. The lights are proposed to be located in landscape areas, spaced between parking lot trees.*

The guidelines does not recommend the following:

- Concrete light fixture bases exceeding 8 inches. *Concrete light bases exceeding 8 inches will not be installed.*
- Parking lot lighting should be designed to avoid unnecessary illumination of residential areas. *Down facing “shoebox” lights are proposed which would prevent illumination from spilling over towards the mixed use residential area across Main Street.*
- Ornamental or contemporary light fixtures which are incompatible with downtown light fixtures. *The light fixture proposed are compatible with the existing parking lot lights within the existing parking lot for Reliable Credit. They are also similar to those parking lot lights in the City parking lot across Harrison from the Reliable Credit site. They will not however be similar to those street lights noted for street lights in either Main or other downtown streets, or for McLoughlin Blvd, all of which are somewhat different. Whether they are incompatible or not is in the eye of the beholder.*
- Parking Lot fixtures taller than 15'. *Parking lots proposed will not exceed 15 feet.*

We note that the guidelines are simply recommendations and do not have the weight as code requirements. While the owners do not have real issues with other style light fixtures, their desire is to have similar lighting throughout their parking area. Regardless of the style of light fixture that may be allowed, recommended or required, the owners do want the new lights to be LED lit. A cut sheet of a proposed “Shoebox” style parking lot light is part of the submittal.

19.607 Off-Street Parking Standards for Residential Areas – *This is not applicable to this application.*

19.608 Loading – *This is not applicable to this application.*

19.609 Bicycle Parking – *Reliable Credit presently has bicycle parking stalls, no new bicycle parking stalls are proposed.*

19.610 Carpool and Vanpool Parking

19.610.1 Applicability – *The development is required to have more than 20 required parking stalls and therefore the code section is applicable.*

19.610.2 Number of Spaces – *The number of carpool/vanpool spaces shall be at least 10% of the minimum amount of required parking spaces. The minimum number of parking space is 44 based on 22,000 SF of floor area, therefore at least 4 carpool/vanpool parking stalls are required. Four existing parking stalls will be signed as carpool/vanpool stalls.*

19.610.3 Location – *Parking for carpool/vanpools shall be located closer to the main entrances of the building than other employee parking, except ADA. The parking stalls immediately in front of the main entrance, other than the ADA stalls, are intended for customers of Reliable Credit. The 4 required designated carpool/vanpool stalls will therefore be located directly north of the drive-up window area approximately equal distant between the main entrance and the employee entrance.*

19.610.4 Standards – *Carpool/vanpool spaces shall be clearly designated with signs or pavement markings for use only by carpools/vanpools. The 4 designated spaces will be marked accordingly.*

19.611 Parking Structures- *This code section is not applicable.*

Chapter 19.700 Public Facility Improvements

19.702 Applicability

19.702.1 General – This section is applicable to the following types of development in all zones.

A) Partitions – *This not applicable to this development as a partition is not proposed.*

B) Subdivisions – *This is not applicable to this development as a subdivision is not proposed.*

C) Replats – *This is not applicable as a replat is not proposed.*

D) New Construction – *This is not applicable as per the definitions in Section 19.201 “new construction” is stated as (1) new structures, (2) new additions to existing structures and (3) reconstruction of fully or partially demolished structures. No structural additions, changes or reconstruction are proposed other than to demolish an existing building.*

E) Modification or expansion of an existing structure or a change or intensification in use. *No modification or expansion of an existing structure is proposed, other than to demolish an existing structure, and no change or intensification in use is proposed. Therefore this section is not applicable.*

Chapter 12.16 Access Management

12.16.030 Access Permitting – *Engineered plans and appropriate permit applications will be made when planning approval of the Land Use Application is granted.*

12.16.040 Access Management Standards

B) Access Spacing

1) Standards – *This is not applicable new access point (for an exit only) will not be on a collector or arterial.*

2) Modification of Access Spacing – *No modification is requested.*

C) Accessway Location

1) Double Frontage – *The site will now have three street frontages to City streets. There is an existing right-in / right-out only entrance and exit onto Harrison; An entrance-only on Main Street; and the proposed new exit-only onto Scott. The new exit will relieve some of the exit movements onto Harrison*

2) Location Limitations – *No new access to a collector or arterial is proposed. All backing movements will be contained on the site.*

3) Distance from Property Line – *The nearest edge of the proposed driveway will be 19.5 feet from the property to the west and 22.5 feet from the property line to the east.*

4) Distance from Intersection

a) *This section is not applicable as the site is not a single family residential property*

b) *At least 100 feet for multifamily residential properties and all other uses accessing local and neighborhood streets. It is not possible to meet this standard as the parcel on which the access exit is to be created is only 55 feet wide and adjacent to the intersection of Scott and Main.*

c) *This section is not applicable as the proposed access is a local street.*

d) *This section is not applicable as the proposed access is a local street.*

D) Number of Accessway Locations

1) Safe Access – *The proposed new parking area will jointly use the existing access point presently serving the site off of main. The exit point is required to serve the one directional traffic flow for this new parking area.*

2) Shared Access – *This is not applicable as there as the proposed new access will be to a local street.*

3) Single Family Residential – *This section is not applicable.*

4) All Uses Other than Single Family Residential

a) *This section is not applicable as the new access will be to a local street.*

b) *One accessway is allowed on local street. One access (exit) is proposed to Scott Street a local street.*

E) Accessway Design

1) Design Guidelines – *The accessway will meet the applicable standards per ADA requirements and the City of Milwaukie.*

3) Backing into the Right-of-Way Prohibited – *Backing into the right-of-way will not be required to serve the proposed parking stalls.*

F) Accessway Size

1) *The accessway will be 13 feet wide to match that of the required drive aisle for one directional 45° degree parking stalls.*

2) *This code section is not applicable as the site is not residential.*

3) *This code section is not applicable as this is not a multi-family development.*

4) *This code section is not applicable as this is not a multi-family development.*

5) *This code section is not applicable as this is not a multi-family development.*

6) *The proposed access (exit) on to Scott Street is proposed to be 13 feet wide exceeding the minimum required width for commercial parcels of 12 feet and below the maximum allowed width of 36 feet.*

7) *This code section is not applicable as this is not an industrial site.*

Chapter 12.24 Clear Vision at Intersections

12.24.030 Requirements

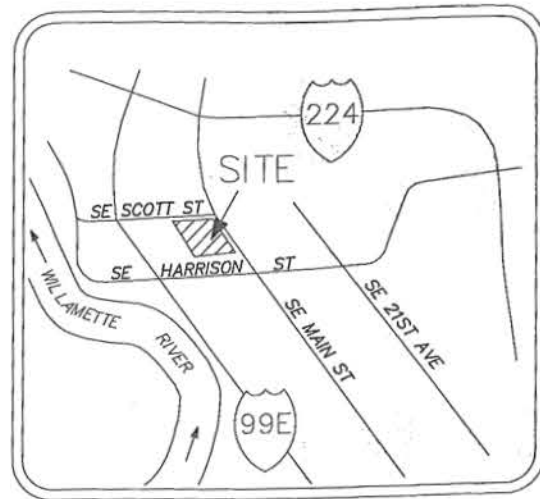
B) A clear vision area shall be maintained at all driveways and accessways and on the corners of all property adjacent to an intersection as provided Section 12.24.040. *The site is located adjacent to a street intersection and will have a new accessway and thus this code section is applicable.*

C) A clear vision area shall contain no plantings, fence, wall structure, or temporary or permanent obstruction, except for an occasional utility pole or tree exceeding 3 feet in height. *This standard will be met.*

12.24.040 Computation

A) The clear vision are for all street intersections shall be that area described in the most recent edition of the “AASHTO Policy on Geometric Design of Highways and Streets”. *Per the Sixth Edition of “AASHTO Policy on Geometric Design of Highways and Streets”, the Design Intersection Sight Distance B1, Left Turn from Stop (Table 9-6) is 225 feet based on an assumed speed limit of 25 mph. As Scott Street is a stop controlled intersection the sight line measure from Scott would be most conservatively measured 14.5 feet to the west of the cross-*

walk line. Where this clear vision triangle crosses the parcel is shown on plan sheets 3 and 4. The clear vision area for all street and driveway or accessway intersections shall be that area within a twenty foot radius from where the lot line and edge of a driveway intersect. The clear vision area for the driveway is shown on plan sheets 3 and 4 as well.



VICINITY MAP
NOT TO SCALE

SURVEYOR

WESTLAKE CONSULTANTS, INC.
15115 SW SEQUOIA PARKWAY, SUITE 150
TIGARD, OR 97224
PHONE: 503.684.0652
FAX: 503.624.0157

BENCHMARK INFORMATION

BRASS PLUG IN THE SIDEWALK ON THE EAST SIDE OF SE MAIN STREET, AS SHOWN ON SURVEY. ASSUMED ELEVATION = 100.00'

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

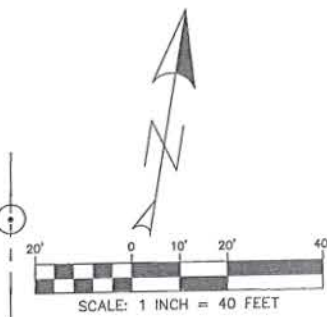
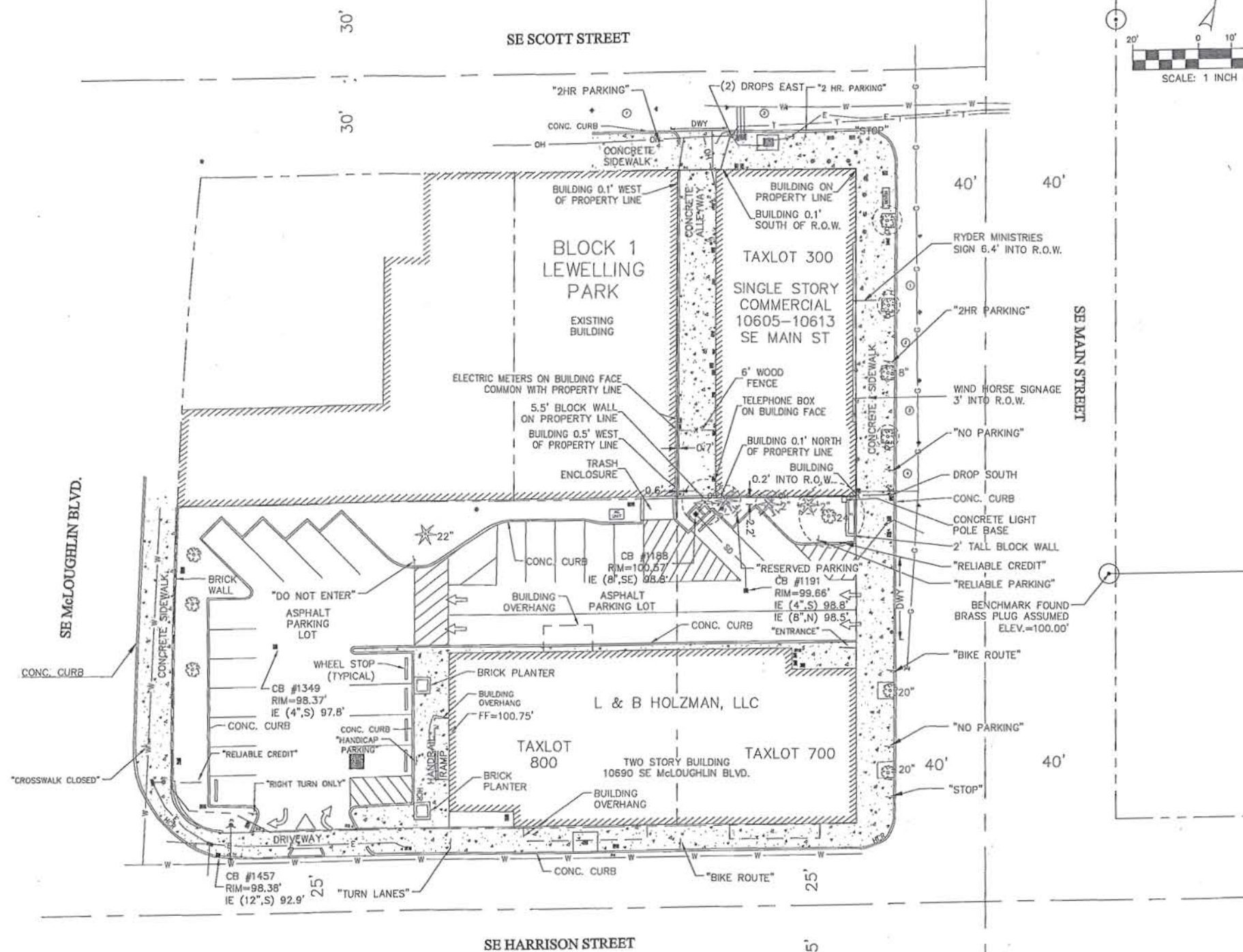
LEGEND

- FOUND MONUMENT AS NOTED
- CLEANOUT
- ⊙ STORM SEWER MANHOLE
- ⊞ CATCH BASIN CENTER
- ▢ AREA DRAIN
- ▢ ROOF DRAIN
- ⊞ WATER VALVE
- ⊞ WATER METER
- ⊞ FIRE HYDRANT
- ⊞ HOSE BIB
- ⊞ ELECTRIC RISER
- ⊞ ELECTRIC VAULT
- ⊞ LIGHT POLE
- ⊞ ELECTRIC METER
- ⊞ GAS METER
- ⊞ GAS VALVE
- ⊞ TELEPHONE RISER
- ⊞ TELEPHONE VAULT
- ⊞ UTILITY POLE
- ⊞ UTILITY BOX
- ⊞ UTILITY VAULT
- ⊞ TRAFFIC SIGNAL BOX
- ⊞ TRAFFIC SIGNAL POLE

- ⊞ SIGN
- ⊞ BIKE RACK
- ⊞ MAIL BOX
- ⊞ GUY ANCHOR
- ⊞ BOLLARD
- ⊞ HANDICAP PARKING SPACE
- ⊞ DECIDUOUS TREE
- ⊞ EVERGREEN TREE

- 12" ⊞
- 24" ⊞

- CONC. CONCRETE
- HCR HANDICAP RAMP
- FF FINISHED FLOOR
- DWY DRIVEWAY
- OH OVERHEAD LINES
- W UNDERGROUND WATER LINE
- E UNDERGROUND POWER
- G UNDERGROUND GAS LINE
- T UNDERGROUND TELE-COM
- SD UNDERGROUND STORM DRAIN LINE
- X FENCE LINE
- /// BUILDING FOOTPRINT LINE



REVISIONS	BY

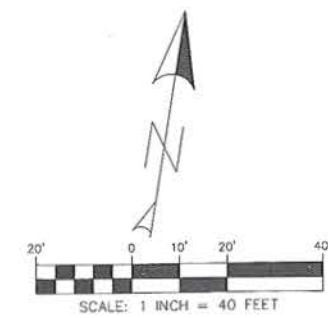
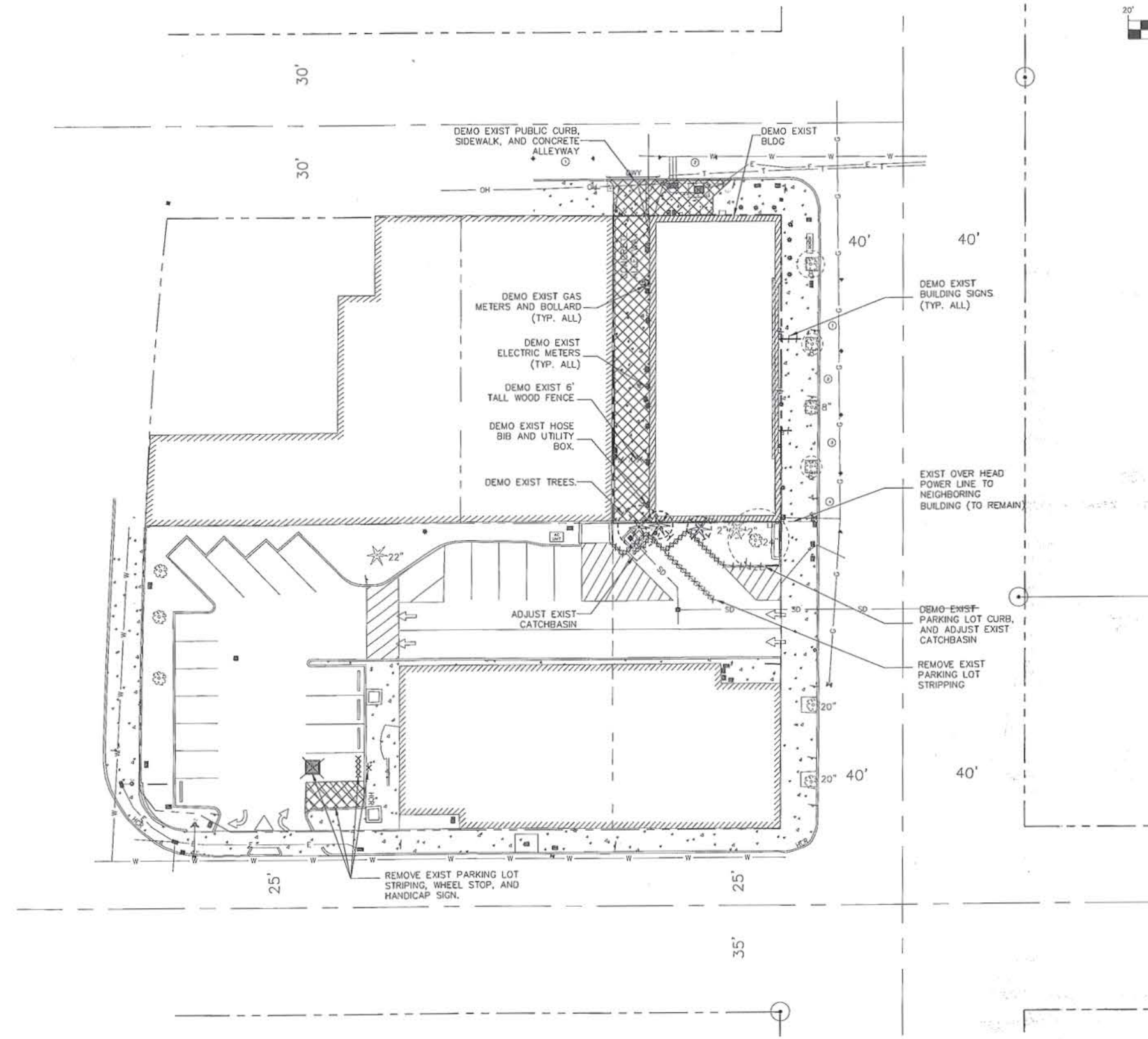
PARKING LOT EXPANSION
L & B HOLZMAN, LLC.

Existing Conditions Plan

ISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 14-033 - EXISTING CONDITIONS.DWG

DATE	SEP. 2014
SCALE	AS NOTED
DRAWN	JMF
JOB	14-033
SHEET	1
OF 5	SHEETS

- LEGEND**
- FOUND MONUMENT AS NOTED
 - CLEANOUT
 - ⊙ STORM SEWER MANHOLE
 - CATCH BASIN CENTER
 - ▣ AREA DRAIN
 - ▤ ROOF DRAIN
 - ⊕ WATER VALVE
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 - FF FINISHED FLOOR
 - DWY DRIVEWAY
 - OH OVERHEAD LINES
 - W UNDERGROUND WATER LINE
 - E UNDERGROUND POWER
 - G UNDERGROUND GAS LINE
 - T UNDERGROUND TELE-COM
 - SD UNDERGROUND STORM DRAIN LINE
 - X FENCE LINE
 - /// BUILDING FOOTPRINT LINE



REVISIONS		BY
1	UPDATED PLAN (10-28-2014)	JVM

PARKING LOT EXPANSION

L & B HOLZMAN, LLC.

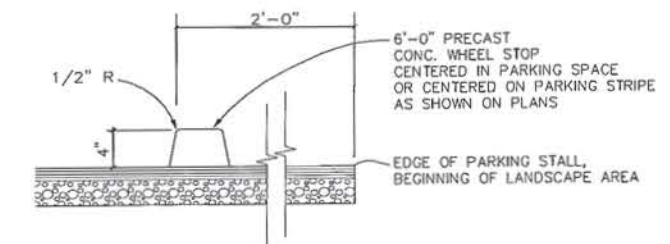
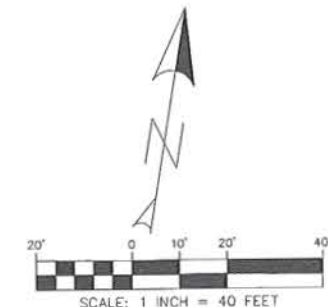
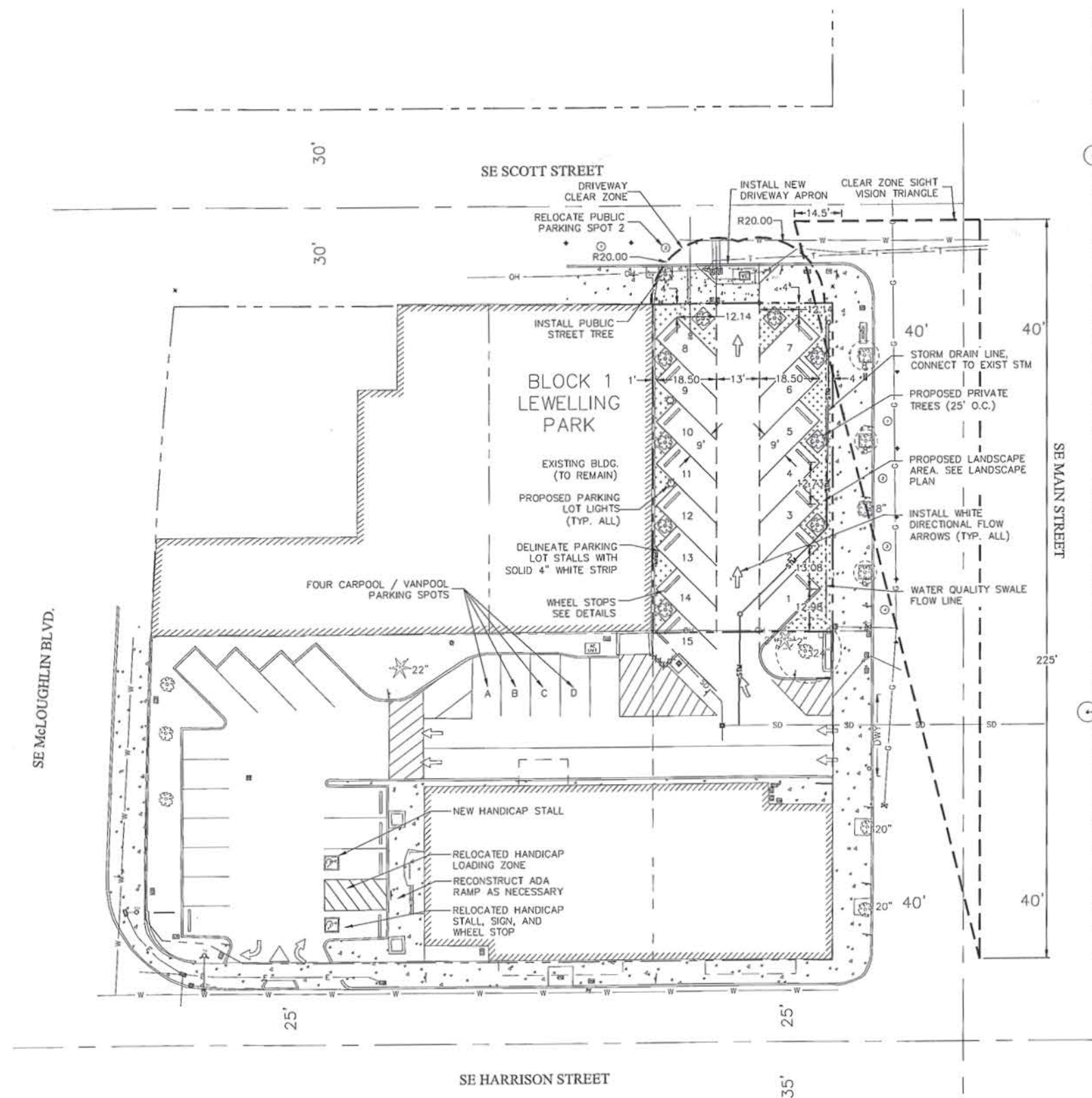
Demolition Plan

SISUL ENGINEERING

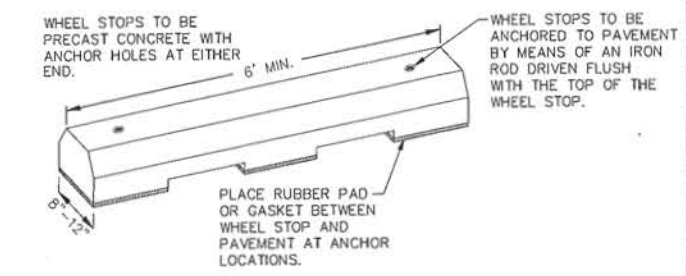
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188

DATE	SEP. 2014
SCALE	AS NOTED
DRAWN	JHF
JOB	14-033
SHEET	2
OF	5 SHEETS

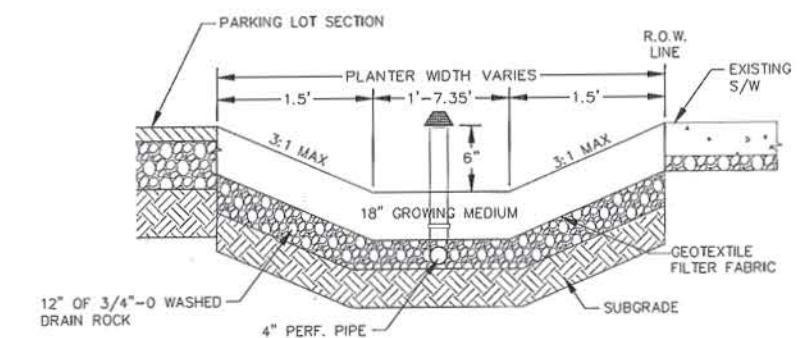
DRAWING: 14-033 - Demo Plan.DWG



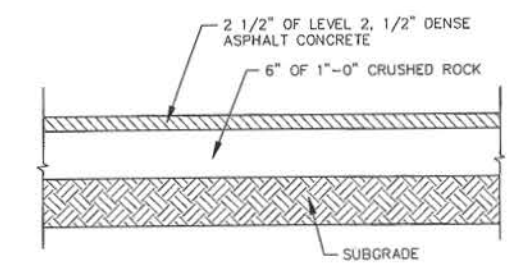
WHEEL STOP PLACEMENT DETAIL
N.T.S.



WHEEL STOP DETAIL



FLOW-THROUGH PLANTER
N.T.S.



TYPICAL PARKING LOT SECTION
N.T.S.

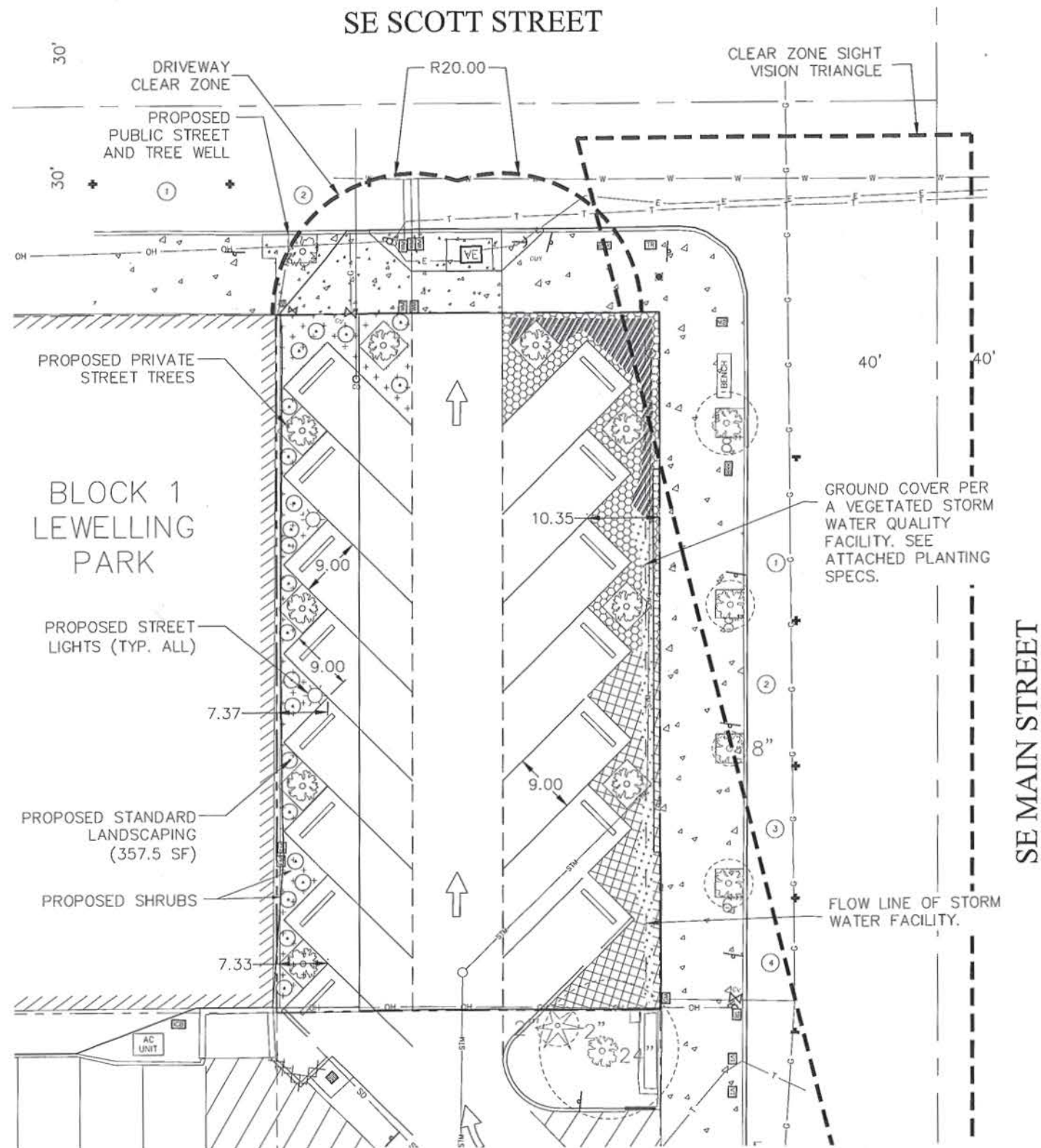
REVISIONS	BY
UPDATED PLAN (10-28-2014)	JVM

PARKING LOT EXPANSION
L & B HOLZMAN, LLC.

Parking Lot Site Plan

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 14-033 - SITE PLANNING

DATE	SEP. 2014
SCALE	AS NOTED
DRAWN	JMF
JOB	14-033
SHEET	3
OF 5	SHEETS

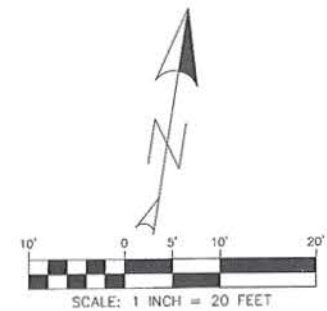


WATER QUALITY SWALE PLANT LIST (EAST SIDES LANDSCAPING)

BOTANICAL NAME	COMMON NAME	MAXIMUM HEIGHT	QUANTITY	SPACING	TYPE
ZONE A: HERBACEOUS PLANTS - 237 SF @ 115 PLANTS PER 100 SF = 273 PLANTS					
JUNCUS PATENS	SPREADING RUSH	36"	136	12" O.C.	
CAREX OBNUPTA	SLOUGH SEDGE	24"	137	12" O.C.	
ZONE B: GROUND COVER AND SMALL SHRUBS - 521 SF @ 12 SMALL SHRUBS AND 70 GROUND COVER PER 100 SF 63 SMALL SHRUBS AND 365 GROUND COVER REQUIRED					
FRAGARIA CHILOENSIS	COSTAL STRAWBERRY	6"	32	12" O.C.	GROUND COVER
MAHONIA NERVOSA	DULL OREGON GRAPE	48"	183	36" O.C.	SMALL SHRUB
ARCTOSTAPHYLOS UVA-URSI	KINNICKINNICK	6"	31	12" O.C.	GROUND COVER
GAULTHERIA SHALLON	SALAL	36"	182	36" O.C.	SMALL SHRUB

LANDSCAPING SYMBOL LEGEND

- PRIVATE LANDSCAPE TREE - ACER RUBRUM 'BOWHALL RED' / BOWHALL RED MAPLE
- PROPOSED SHRUB - PRUNUS LAUROCERASUS 'OTTO LUKEN' / LUKENS LAUREL



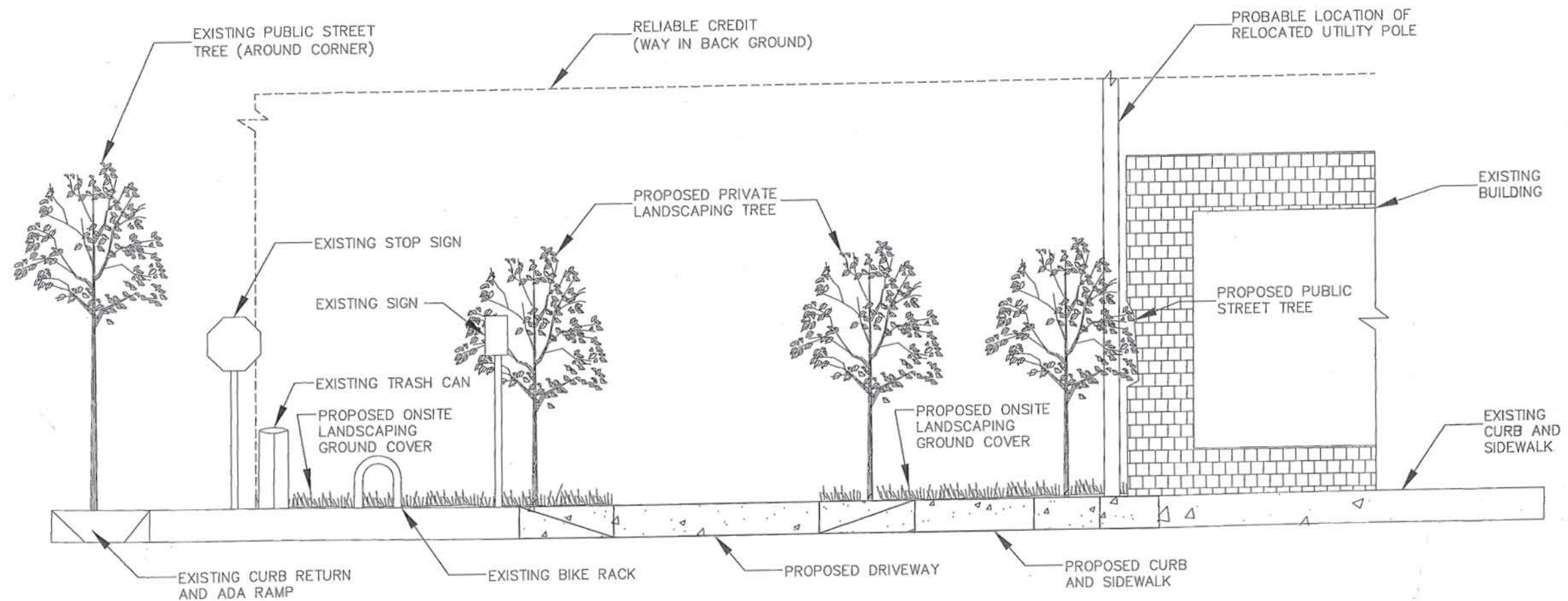
PARKING LOT EXPANSION
L & B HOLZMAN, LLC.

Landscape Plan

SISUL ENGINEERING

375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 14-033 - SITE PLANNING

DATE SEP. 2014
SCALE AS NOTED
DRAWN JHF
JOB 14-033
SHEET 4
OF 5 SHEETS



LOOKING SOUTH FROM SE SCOTT STREET

SCALE 1" = 4'

PARKING LOT EXPANSION
L&B HOLZMAN, LLC.

Elevation View

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 14-033 ELEVATION.DWG

DATE SEP. 2014
SCALE AS NOTED
DRAWN JMF
JOB 14-033

SHEET
5
OF 5 SHEETS

REVISIONS	BY



FEATURES

Die Cast Aluminum Housing & Hinged Front Frame, ½" Coin Plugs for Conduit & Photocell, Textured Architectural Bronze Powdercoat Finish Over a Chromate Conversion Coating

Clear Flat Glass Lens or Clear Flat Prismatic Glass Lens

Mount with Extruded Mounting Arms.

Custom Colors Available

LumaFit LED:

Array Lumens: 5338; CRI: 80+; CCT: 5000K

Rated Life: 50,000 Hours; 5-Year Warranty

Aluminum Boards

Wattage:

Array 55.8w, System 62w

Driver:

Electronic Driver, 120-277V, 50/60Hz

Dimmable Driver

Listing & Ratings:

CSA: Listed for Wet Locations

Options:

2.5KV Surge Protection

Accessories Sold Separately

ORDER INFORMATION

EasyLED 12" Area Light

Catalog #	Description
Complete Units	
Ordering Information	
Example: 12ALQF1X56U5KCZMA18RSP	
12AL	Model: 12AL
Q	LED Mfr: Q=QSSI
F	Optics: C=Type 3, F=Medium Beam Spread
1X	Number of Arrays: 1X=One
56	Wattage: 56=56w
U	Ballast: U=120-277V
5K	CCT: 5K=5000K
C	Lens: C=Clear Flat Glass Lens, P=Clear Flat Prismatic Glass Lens* *Use with F Optic Only.
Z	Color: Z=Bronze
MA18R	Mounting: MA6*=6" Aluminum Extruded Mounting Arm, MA10*=10" Aluminum Extruded Mounting Arm, MA12*=12" Aluminum Extruded Mounting Arm, *Add R for Round Pole Mounting, Fits 4" Diameter Poles.
SP	Options: Specify Twist Lock Photocell: P40=P18140 110-120VAC Instant, P42=P18142 110-277VAC Instant, P50=P18150 120VAC Time Delay, P52=P18152 277VAC Time Delay SF=Single Fuse, DF=Double Fuse, SP=Surge Protection

Complete Units

Ordering Information

Example: 12ALQF1X56U5KCZMA18RSP

12AL	Model:	12AL
Q	LED Mfr:	Q=QSSI
F	Optics:	C=Type 3, F=Medium Beam Spread
1X	Number of Arrays:	1X=One
56	Wattage:	56=56w
U	Ballast:	U=120-277V
5K	CCT:	5K=5000K
C	Lens:	C=Clear Flat Glass Lens, P=Clear Flat Prismatic Glass Lens* *Use with F Optic Only.
Z	Color:	Z=Bronze
MA18R	Mounting:	MA6*=6" Aluminum Extruded Mounting Arm, MA10*=10" Aluminum Extruded Mounting Arm, MA12*=12" Aluminum Extruded Mounting Arm, *Add R for Round Pole Mounting, Fits 4" Diameter Poles.
SP	Options:	Specify Twist Lock Photocell: P40=P18140 110-120VAC Instant, P42=P18142 110-277VAC Instant, P50=P18150 120VAC Time Delay, P52=P18152 277VAC Time Delay SF=Single Fuse, DF=Double Fuse, SP=Surge Protection

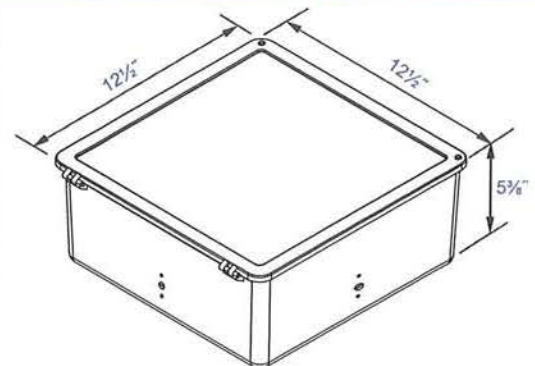
EasyLED 12" Area Light



12AL



DIMENSIONS



12AL

APPLICATIONS

Auto Dealerships
Parking Lots
Retail Malls
Shopping Centers
Commercial & Industrial Complexes
Walkways, Driveways & Parkways

LED



EasyLED 12" Area Light

ACCESSORIES

* Add R for Round Pole Mounting,
Fits 4" Diameter Poles.

PHOTOCELLS:

P18130 - 480VAC Twist Lock Photocell
Receptacle

P18131 - Twist Lock Non Shorting
(Open) Cap disconnects service to
fixture for temporary or permanent
disabling (fixture always off). IP65, 480V
maximum.

P18132 - Twist Lock Shorting Cap
provides fixed service to fixture (fixture
always on). IP65, rated load 7200w
Tungsten.

P18140 - 110-120VAC Instant Twist Lock

P18142 - 110-277VAC Instant Twist Lock

P18150 - 120VAC Time Delay Twist Lock

P18152 - 277VAC Time Delay Twist Lock



**Glare Shield
(12ALGS)**



**6" Mounting Arm
(MA6")**



**10" Mounting Arm
(MA10")**



**12" Mounting Arm
(MA12")**



**Pole Tenon Adaptor
(PTASUNV)**



**Wall Bracket
(FLEMWM)**



P18130



P18131



P18132



P18140



P18142



P18150



P18152

PHOTOMETRICS

Specifications subject to change without notice.



Existing Parking Lot Light
at Elia le redit

***RELIABLE CREDIT
PARKING LOT EXPANSION***

J.O. SGL 14-033

L&B HOLZMAN, LLC.

October 28, 2014

***PRELIMINARY STORM DRAIN
PLANTER CALCULATIONS***

SISUL ENGINEERING

A Division of Sisul Enterprises, Inc.

375 Portland Avenue

Gladstone, OR 97027

phone: (503) 657-0188

fax: (503) 657-5779

Project overview and Description:

The site is located at 10613 SE Main St and is currently developed. The site improvements will include the demolition of an existing building and replaced with a new parking lot.

A flow through planter will be used to meet the storm drain requirements. It will be located on the east side of the site. The parking lot will sheet flow into the planter through curb cuts in the curb. The overflow pipe for the planter will drain to an existing storm drain line already on-site.

Areas:

Total Site Area = 4,251 sf

Impervious Area

Parking Lot & Sidewalk Area = 4,251 sf

Total Impervious = 4,251 sf

Methodology:

Existing Drainage of the site:

The site currently has an existing building that will be demolished and replaced with a new parking.

Proposed Drainage of the site:

The developed site will drain to the east, into the proposed flow through planter. The overflow pipe for the planter will drain to an existing storm drain line already on-site.

Infiltration results:

With a flow through planter being used for water quality and detention on the site, an infiltration rate of 2 inches per hour was used for calculations.

Stormwater Hierarchy Category justification:

Hierarchy category 1, requires total on-site infiltration with vegetated infiltration facilities. Category 1 is not possible due to the low infiltration rate of 0.1 inches per hour.

Hierarchy category 2, requires total on-site infiltration with vegetated infiltration facilities that overflow to subsurface infiltration facilities. Category 2 is not possible due to the low infiltration rate of 0.1 inches per hour.

Hierarchy category 3, requires on-site detention with vegetated facilities that overflow to a drainageway, river or storm-only pipe. Category 3 will be used with a vegetated flow through planter that overflows and will be tied into an existing storm drain line located on the southern portion of the property.

Analysis:

Design Assumptions:

- 1.) The PAC calculator will be used to size the infiltration facilities.
- 2.) The time of concentration for post development is 5 minutes.
- 3.) A CN of 98 will be used for determination of post development peak flow.

Escape Route:

If the basin overflows, it will drain into the public street and into the public storm drain system on SE Main Street.

PAC Calculator Calculation Description:

The PAC Calculator will be used to size the vegetated flow through planter. The planter will have 4" of surface storage and 2" of freeboard. The growing medium will be 18" deep and the drain rock below the growing medium will be 12" deep. The landscape and bottom width of the facility will varies throughout the flow through planter. For calculation purposes an average landscape width of 7' and a bottom width 4' will be used running the PAC Calculator.

Engineering Conclusions

The proposed stormwater facility has been designed in accordance with the 2014 Storm Water Management Manual. The storm water facility has been designed to meet flow control and pollution control requirements according to the PAC Calculator. See the attached PAC calculations. According to the calculations, the vegetated flow through planter will use 13% of the surface storage for pollution reduction. To meet the flow control requirements, the planter will reduce the post developed runoff to the pre-developed runoff rates for the 2, 5, 10 and 25 year storm events as summarized in the table below.

Storm Event	Pre-Developed Runoff	Post-Developed Runoff
2-year	0.060	0.049
5-year	0.073	0.062
10-year	0.087	0.076
25-year	0.100	0.089

Per the table above, the post developed runoff is less than the pre developed runoff.



Presumptive Approach Calculator ver. 1.2

Catchment Data

Project Name: **Reliable Credit Parking Lot**

Project Address: **10605-10613 SE Main St**

Milwaukie, Oregon

Designer: **JVM**

Company: **Sisul Engineering**

Catchment ID: **A**

Date: **10/28/14**

Permit Number: **0**

Run Time 10/29/2014 9:03:46 AM

Drainage Catchment Information

Catchment ID	A
Catchment Area	4,251 SF
Impervious Area	0.10 ac
Impervious Area Curve Number, CN_{imp}	98
Time of Concentration, T_c , minutes	5 min.

Site Soils & Infiltration Testing Data

Infiltration Testing Procedure:	Open Pit Falling Head
Native Soil Field Tested Infiltration Rate (I_{test}):	4 in/hr
Bottom of Facility Meets Required Separation From High Groundwater Per BES SWMM Section 1.4:	Yes

Correction Factor Component

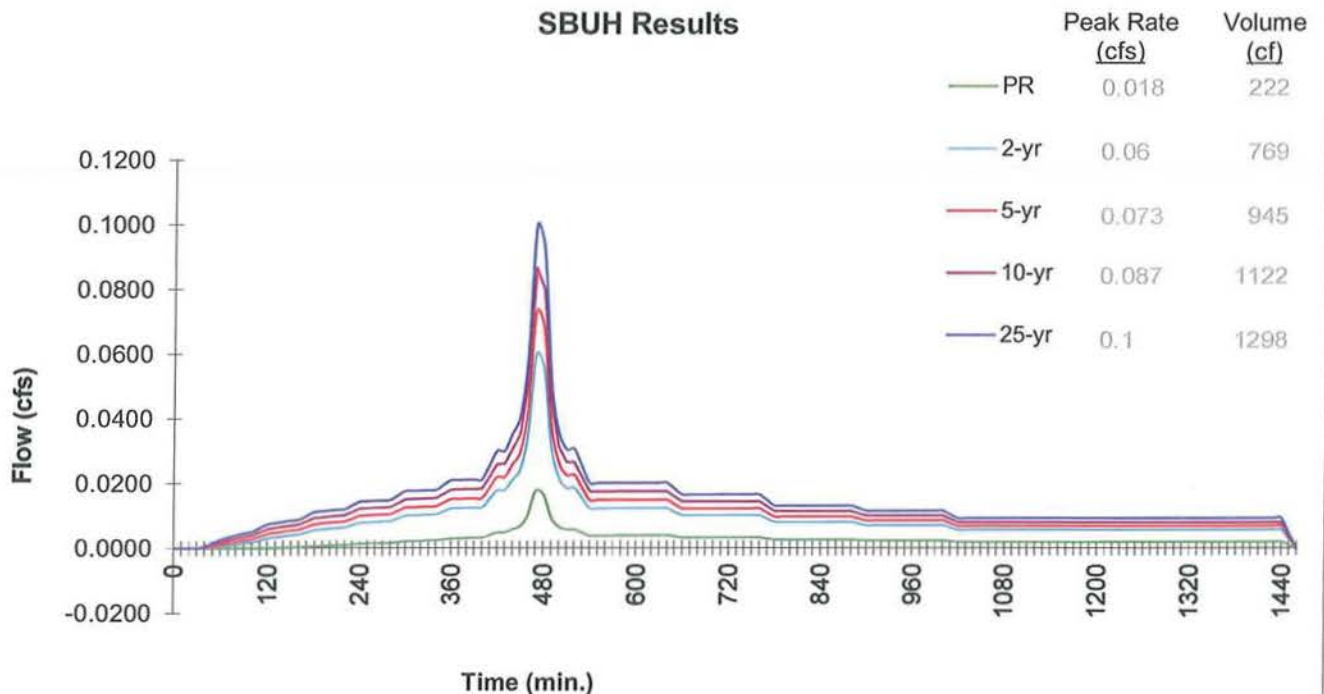
CF_{test} (ranges from 1 to 3)	2
----------------------------------	----------

Design Infiltration Rates

I_{dsgn} for Native (I_{test} / CF_{test}):	2.00 in/hr
I_{dsgn} for Imported Growing Medium:	2.00 in/hr

Execute SBUH Calculations

SBUH Results





Presumptive Approach Calculator ver. 1.2

Catchment ID: **A**

Run Time 10/29/2014 9:03:46 AM

Project Name: Reliable Credit Parking LotCatchment ID: ADate: 10/28/2014

Instructions:

1. Identify which Stormwater Hierarchy Category the facility.
2. Select Facility Type.
3. Identify facility shape of surface facility to more accurately estimate surface volume, except for Swales and sloped planters that use the PAC Sloped Facility Worksheet to enter data.
4. Select type of facility configuration.
5. Complete data entry for all highlighted cells.

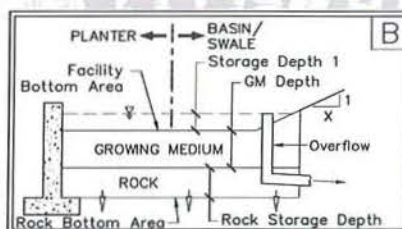
Catchment facility will meet Hierarchy Category: **3**

Goal Summary:

Hierarchy Category	SWMM Requirement	RESULTS box below needs to display...	
		Pollution Reduction as a	10-yr (aka disposal) as a
3	Off-site flow to drainage way, river, or storm-only pipe system.	PASS	N/A

Facility Type = **Planter (Sloped)**

Click Shape

Facility Configuration: **B**

Refer to Sloped Facility
Worksheet and enter
Variable Parameters

DATA FOR ABOVE GRADE STORAGE COMPONENT

Infiltration Area = **238** sf
Surface Capacity Volume = **55.6** cf

BELOW GRADE STORAGE

Rock Storage Bottom Area = **736** sf
Rock Storage Depth = **12** in
Rock Void Ratio = **0.3**

Growing Medium Depth = **18** in
Freeboard Depth = **N/A** in

Surface Capacity at Depth 1 = **56** cf
Infiltration Area at 75% Depth1 = **47** SF
GM Design Infiltration Rate = **2.00** in/hr
Infiltration Capacity = **0.011** cfs

Rock Storage Capacity = **221** cf

Native Design Infiltration Rate = **2.00** in/hr
Infiltration Capacity = **0.034** cfs

Calculation Guide

Max. Rock Stor.
Bottom Area
Per Swale Dims

RESULTS		Overflow Volume	
Pollution Reduction	PASS	0 CF	13% Surf. Cap. Used
			0% Rock Cap. Used
Run PAC			
Output File			
Peak cfs	2-yr 0.049	5-yr 0.062	10-yr 0.076
		25-yr 0.089	

FACILITY FACTS

Total Facility Area Including Freeboard = **644 SF**
Sizing Ratio (Total Facility Area / Catchment Area) = **0.151**



Presumptive Approach Calculator Ver 1.2

Instructions:

- 1. Refer to facility graphics on the Graphics tab, then fill in all relevant facility parameters in the Data Entry table below. Data entry cells vary based on Facility Configuration selected on Facility Design Data tab.
- 2. Delete all facility parameters that may have been entered by the previous iteration that are no longer applicable.

Project Name: Reliable Credit Parking Lot

Date: 10/28/2014

Run Time 10/29/2014 8:03:45 AM

Catchment ID: A

Data Entry

Parameters									Rock Storage Parameters		
Facility Segment	Length of facility segment (ft)	Downstream Check Dam Length (ft)	Longitudinal Facility Slope (ft/ft)	Bottom Width (ft)	Side Slope Right	Side Slope Left	Downstream Depth (inches)	Landscape Width (ft)	Rock Storage Width (ft)	Rock Storage Depth (inches)	Rock Void Ratio
	L _{segment}	L _{dam}	S	W _{bottom}	X _{right} :1	X _{left} :1	D _{ds}	W _{landscape}	W _{rock}	D _{rock}	v
1	92	0	0.005	4	3	3	4	7	8	12	0.3
2									8		
3									8		
4									8		
5									8		
6									8		
7									8		
8											
9											
10											
11											
12											
13											
14											
15											
16											
17											
18											
19											
20											

Error Messages

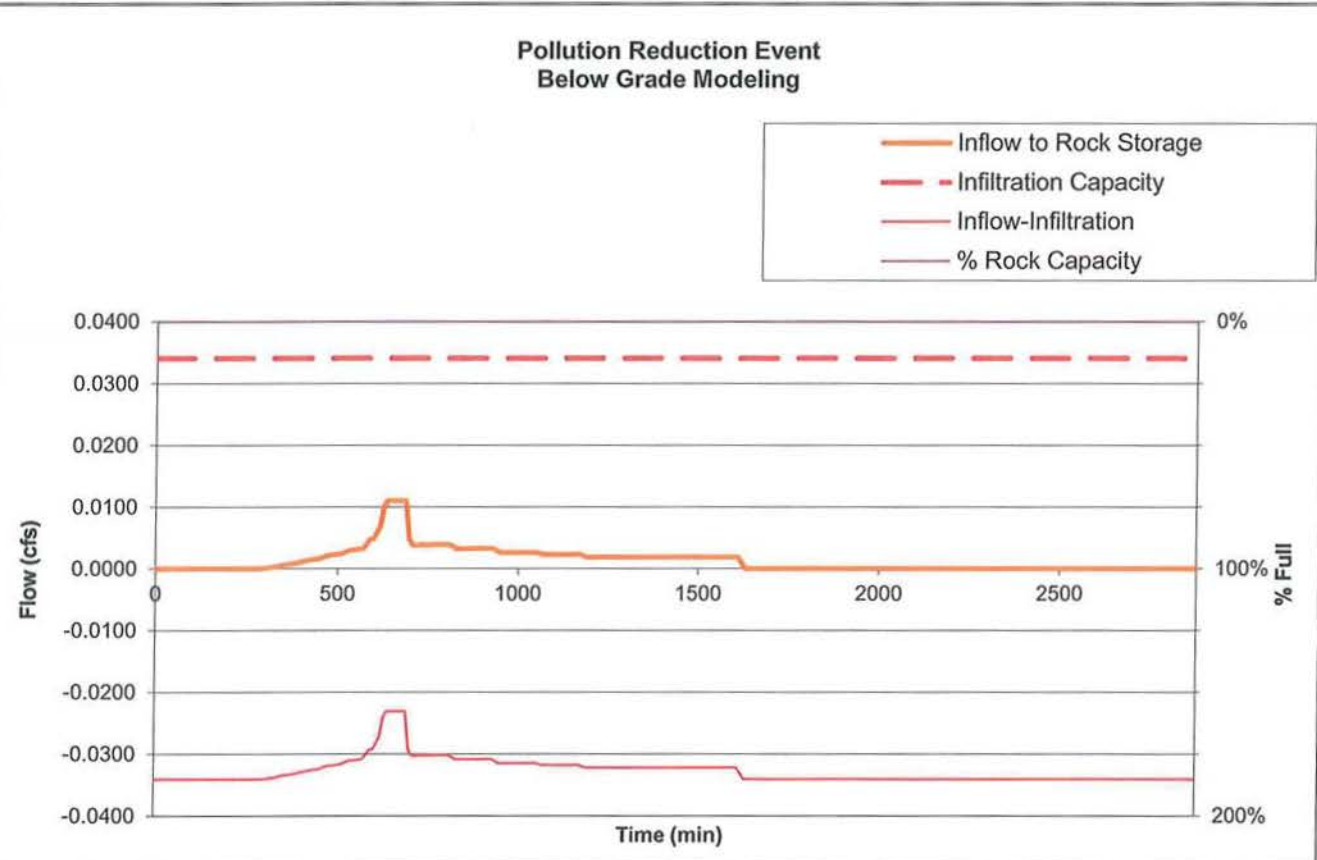
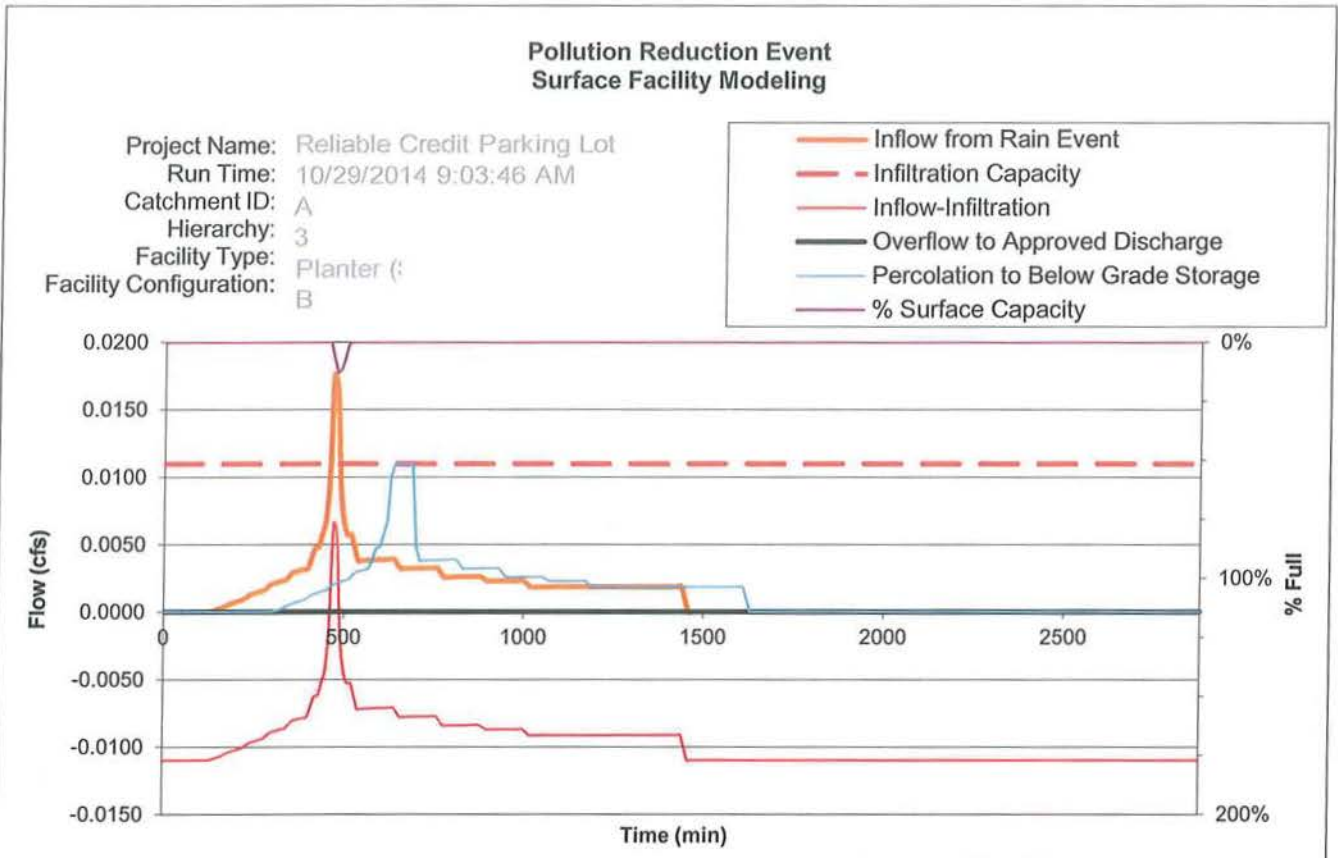
facility segment with warning message not fully utilized. Create shorter facility segments to increase surface storage capacity and infiltration area.

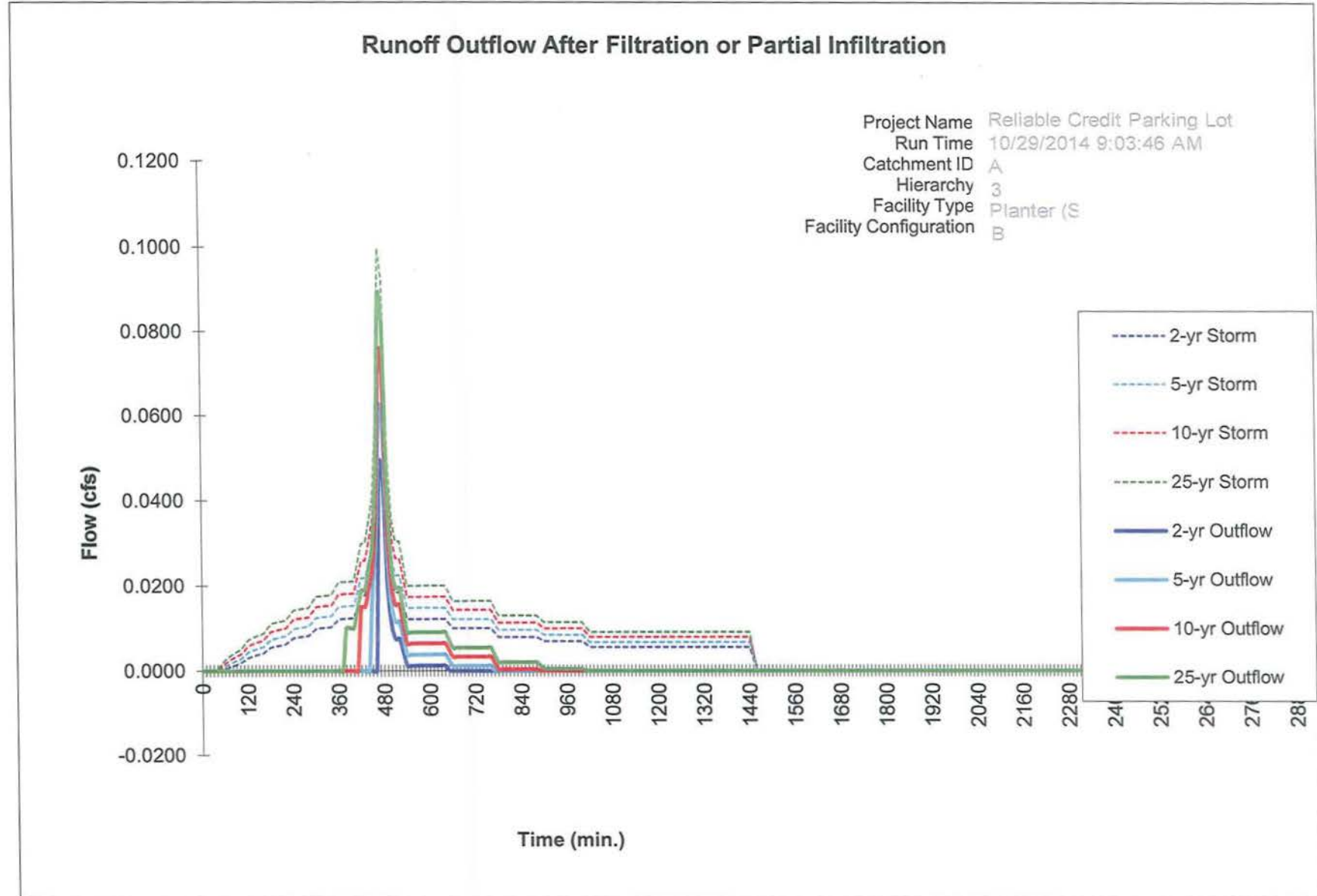
Warning

Project Name: _____ Depth 2= _____ Depth 3= _____

Worksheet Calculations

Parameters															Rock Storage Parameters		
Facility Segment	Adjusted Length of facility segment (ft)	Adjusted Length if D _{up} = 0 (ft)	Upstream Depth (inches)	Downstream Top Width (ft)	Upstream Top Width (ft)	Downstream Cross-sectional Area (sf)	Upstream Cross-sectional Area (sf)	Surface Capacity Volume (cf)	75% of Max. Downstream Depth (inches)	75% of Max. Upstream Depth (inches)	75% of Max. Adjusted Length if D _{up75%} = 0 (ft)	75% of Max. Downstream Top Width (ft)	75% of Max. Upstream Top Width (ft)	Infiltration Area @ 75% Full (sf)	Rock Storage Length (ft)	Rock Storage Bottom Area (sf)	Rock Storage Capacity Volume (cf)
	L _{adjust}	L _{adjust2}	D _{up}	W _{top-ds}	W _{top-up}	A _{ds}	A _{up}	V _{surface}	D _{ds75%}	D _{up75%}	L _{adjust3}	W _{top-ds75%}	W _{top-up75%}	A _{75%}	L _{rock}	A _{rock}	V _{rock}
1	92.00	66.67	0.00	6.00	4.00	1.67	0.00	56	3.00	0.00	50.00	5.50	4.00	238	92	736	221
2	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
3	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
4	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
5	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
6	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
7	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
8	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
9	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0	0.00	0.00	0.00	0.00	0.00	0	0	0	0
56 V _{surface} @ Depth1															238	736	221





CITY OF MILWAUKIE

PreApp Project ID #: 14-006PA

PRE-APPLICATION CONFERENCE REPORT

This report is provided as a follow-up to a meeting that was held on **6/5/2014** at **10:00 AM**

Applicant Name: TOM SISUL
Company: SISUL ENGINEERING
Applicant 'Role': Other
Address Line 1: 375 PORTLAND AVE
Address Line 2:
City, State Zip: GLADSTONE OR 97027
Project Name:
Description:
ProjectAddress: 10605 SE MAIN STREET
Zone: Downtown Commercial (DC)
Occupancy Group:
ConstructionType:
Use: Proposal is to demolish the building located at 10605 SE Main Street to construct accessory parki
Occupant Load:
AppsPresent: Tom Sisul & Lee Holzman
Staff Attendance: Steve Butler, Brad Albert, Li Alligood, Vera Kolias, John Stelzenmueller

BUILDING ISSUES

ADA: Building permit will be required to verify ADA van accessible parking requirements.
Structural: Environmental hazardous materials report required.
Demolition permit required.
Mechanical:
Plumbing:
Plumb Site Utilities: Plumbing permit required for cap off of existing systems and for any proposed catchbasins for stormwater management.
Electrical:
Notes: Erosion control permit required.

Please note all drawings must be individually rolled. If the drawings are small enough to fold they must be individually folded.

FIRE MARSHAL ISSUES

Fire Sprinklers:

Fire Alarms:

Fire Hydrants:

Turn Arounds:

Addressing:

Fire Protection:

Fire Access:

Hazardous Mat.:

Fire Marshal Notes: Not at this moment

PUBLIC WORKS ISSUES

Water: N/A

Sewer: N/A

Storm: Submission of a storm water management plan by a qualified professional engineer is required as part of the proposed development. The plan shall conform to Section 2 - Stormwater Design Standards of the City of Milwaukie Pubic Works Standards.
The storm water management plan shall demonstrate that the post-development runoff does not exceed the pre-development, including any existing storm water management facilities serving the development property. Also, the plan shall demonstrate compliance with water quality standards. The City of Milwaukie has adopted the City of Portland 2008 Stormwater Management Manual for design of water quality facilities.
All new impervious surfaces, including replacement of impervious surface with new impervious surfaces, are subject to the water quality standards. See City of Milwaukie Public Works Standards for design and construction standards and detailed drawings.

The storm SDC is based on the amount of new impervious surface constructed at the site. One storm SDC unit is the equivalent of 2,706 square feet of impervious surface. The storm SDC is currently \$765 per unit. The storm SDC will be assessed and collected at the time the building permits are issued.

Street: N/A

Frontage: N/A

Right of Way: N/A

Driveways: Code Section 12.16.040.A states that access to private property shall be permitted with the use of

driveway curb cuts and driveways shall meet all applicable guidelines of the Americans with Disabilities Act (ADA). Driveway approaches shall be improved to meet the requirements of Milwaukie's Public Works Standards.

A driveway access onto SE Scott Street shall be located 45 feet from SE Main Street. The existing alley access shall be closed in accordance with the Public Works Standards.

Erosion Control: Per Code Section 16.28.020(C), an erosion control permit is required prior to placement of fill, site clearing, or land disturbances, including but not limited to grubbing, clearing or removal of ground vegetation, grading, excavation, or other activities, any of which results in the disturbance or exposure of soils exceeding five hundred square feet.

Code Section 16.28.020(E) states that an erosion control permit is required prior to issuance of building permits or approval of construction plans. Also, Section 16.28.020(B) states that an erosion control plan that meets the requirements of Section 16.28.030 is required prior to any approval of an erosion control permit.

Traffic Impact Study: N/A

PW Notes: N/A

PLANNING ISSUES

Setbacks: N/A: Downtown Commercial zone – accessory parking lot to bank building.

Landscape: : Lot line abutting ROW: 4' perimeter landscape strip; 1 tree per 40 lineal feet required. Interior landscaping: minimum 25 SF of interior landscaping/parking spot. Planting areas must be minimum 120 SF in area; minimum 6 feet wide. Shall be either a divider median or a landscape island in the middle or at the end of a parking row. At least one tree per island. Curb overhangs over adjacent landscape areas are not allowed.

MMC Section 19.606.2

MMC Section 19.606.3 – Additional Design Standards

Parking: General office, including banks: Min. 2 per 1,000 SF; Max 3.4 per 1,000 SF See MMC Section 19.605 for more details. All dimensions within Table 19.606.1 must be met in the parking lot design or a variance must be requested (see variance information below).

Transportation Review: The City's transportation requirements are located in MMC 19.700. The Engineering Department has determined that this chapter will not be triggered by the proposed project.

Application Procedures: The applicant is interested in the demolition of an existing building and the construction of a new parking lot for employees. Application for and receipt of a demolition permit will be a condition of approval for this land use application.

Application procedures are described below.

Downtown Design Review (Type III): The application is reviewed through a Type III review per MMC 19.1006, and the application fee is \$2,000. The following sections of the Milwaukie Municipal Code apply to Downtown Design review: 19.907 Downtown Design Review. Milwaukie Downtown Design review guidelines.

Application fees are based on the current fee schedule. Fees are typically updated on July 1st of each year.

For the City's initial review, the applicant should submit 5 complete copies of the application, including all required forms and checklists. A determination of the application's completeness will be issued within 30 days. If deemed incomplete, additional information will be requested. If deemed complete, additional copies of the application will be required for referral to other departments, the Neighborhood District Association (NDA), and other relevant parties and agencies. City staff will inform the applicant of the total number of copies needed.

Land use application submission materials are listed below for your convenience. Please refer to the handouts distributed at the pre-application conference for more detailed information.

1. All applicable land use applications forms with signatures of property owners.
2. All applicable land use application fees.
3. Completed and signed "Submittal Requirements".
4. 5 copies of an existing conditions and a proposed conditions site plan, both to scale. Once the application is deemed complete, additional copies will be requested for distribution to City departments, applicable governmental agencies, and the neighborhood district association for review.

Type III applications are quasi-judicial in nature and are decided by the Planning Commission at a public hearing. The Planning Commission hears land use applications on the second and fourth Tuesdays of every month, and completed applications need to be submitted to the Planning Department no later than 45 days prior to the target Planning Commission hearing. In general, staff recommends that applications be submitted one to two weeks before the 45-day deadline in order to ensure that there is time to make the applications complete if they are initially deemed incomplete. Once the Planning Commission renders a decision, there is a fifteen calendar-day appeal period. Building permits will be accepted for review only after the appeal period for all land use decisions has expired.

Variance application (Type III): MMC 19.911 describes the applicability and review process for variances. Variances are processed through either a Type II or Type III process per 19.911.3. Type II variance requests are listed in 19.911.3.B; a request to vary the dimensions of parking spaces is not listed in 19.911.3.B and is thus subject to a Type III review process as noted in 19.911.3.C.

The application is reviewed through a Type III review per MMC 19.1006, and the application fee is \$2,000. (A 25% discount is applied to two or more applications which relate to the same unit of land and which will be reviewed and decided concurrently.) Approval criteria for Type III variances are identified in MMC 19.911.4.B. All of the criteria in either Subsection 19.911.4.B.1 or 2 must be met. The applicant may choose which set of criteria to meet based upon the nature of the variance request, the nature of the development proposal, and the existing site conditions.

See discussion above regarding Type III review application requirements.

Natural Resource Review: The property does not contain any mapped natural resource areas.

Lot Geography: The site is rectilinear in shape.

Planning Notes:

1. The pre-application conference is valid for purposes of submitting future land use applications as described in MMC 19.1002.4. A preapplication conference is valid for 2 years. Construction of new single-family homes on the newly created lots will require another pre-application conference.
2. The site is located in the Historic Milwaukie Neighborhood District Association (NDA) boundary. Staff strongly encourages the applicant to present any proposed Type III application to the NDA and/or

its Land Use Committee, as well as to the immediate property owners. The NDA's webpage is on-line at <http://www.milwaukieoregon.gov/citymanager/historic-milwaukie-nda>. Their meetings are held at 6:30pm on the second Monday of the month at the Ledding Library Pond House located at 22nd & Harrison. The NDA Chairperson is Dion Shepard (503-653-6207, sheparddioni@hotmail.com). Please contact the Chair to coordinate a meeting to discuss the proposal.

ADDITIONAL NOTES AND ISSUES

County Health Notes:

Other Notes:

This is only preliminary preapplication conference information based on the applicant's proposal and does not cover all possible development scenarios. Other requirements may be added after an applicant submits land use applications or building permits. City policies and code requirements are subject to change. If you have any questions, please contact the City staff that attended the conference (listed on Page 1). Contact numbers for these staff are City staff listed at the end of the report.

Sincerely,

City of Milwaukie Development Review Team

BUILDING DEPARTMENT

Mark Ennis - Plans Examiner - 971-344-0182

Bonnie Lanz - Permit Specialist - 503-786-7613

ENGINEERING DEPARTMENT

Jason Rice - Engineering Director - 503-786-7605

Brad Albert - Civil Engineer - 503-786-7609

Adriana Slavens - Civil Engineer - 503-786-7602

Alex Roller - Engineering Technician I - 503-786-7695

COMMUNITY DEVELOPMENT DEPARTMENT

Stephen Butler - Comm. Dev. Dir. - 503-786-7652

Marcia Hamley - Admin Specialist - 503-786-7656

Alicia Martin -Admin Specialist - 503-786-7600

Blanca Marston -Admin Specialist - 503-786-7600

PLANNING DEPARTMENT

Dennis Egner - Planning Director - 503-786-7654

Brett Kelter - Associate Planner - 503-786-7657

Li Alligood - Associate Planner - 503-786-7627

Vera Kolias - Associate Planner - 503-786-7653

CLACKAMAS FIRE DISTRICT

Mike Boumann - Lieutenant Deputy Fire Marshal - 503-742-2673







Clackamas County Fire District #1

Fire Prevention Office



E-mail Memorandum

To: Vera Kolias, Associate Planner, City of Milwaukie Planning Department

From: **Mike Boumann**, Deputy Fire Marshal, Clackamas Fire District #1

Date: **11/21/2014**

Re: **10605 SE Main Street**

This review is based upon the current version of the Oregon Fire Code (OFC), as adopted by the Oregon State Fire Marshal's Office. The scope of review is typically limited to fire apparatus access and water supply, although the applicant must comply with all applicable OFC requirements. When buildings are completely protected with an approved automatic fire sprinkler system, the requirements for fire apparatus access and water supply may be modified as approved by the fire code official. The following items should be addressed by the applicant:

COMMENTS:

1. The Fire District has no comments for this proposal.

Historic Milwaukie NDA Minutes

Location: Pond House, Milwaukie OR

Date & Time: July 14, 2014 6:30pm

City Liaison: Katie Newell

Members Attending: 15

Chief Bartol gave an update on the police department.

- K9 dog walk fundraiser on Saturday, July 19th. Start times 9am, 10am, & 11am
- Dine out Tuesday, July 15th, ChaChaCha's will donate 15% to Milwaukie police department canine program
- Homecoming tribute planned for wounded Milwaukie soldier, Alex Hussey, on Saturday, July 26th at 5pm.

North Clackamas Parks & Recreation District, Fiona Gwozdz, presented the new draft master plan

- Weigh in on the new master plan! Explore their virtual open house at ncprd.com
- Public meeting on November's ballot measure in Oregon City on Thursday, July 17th

Reliable Credit new employee parking lot project, Tom Sisul & Lee Holzman, asked for the support of the neighborhood association.

- New employee parking lot (15) 8.5 feet parking spots
- Entrance on Main and exit on Scott
- Buildings on Main Street will be removed for this project

Katie Newell's City update

- Foreign Film Night on the 3rd Thursday of the month at the Pond House.
- Moving Forward Milwaukie project team is working on the Action & Implementation Plan with the council. The first Planning Commission hearing tentatively scheduled for mid-October.
- The Moving Forward Milwaukie & Monroe Street Neighborhood Green Concept Plan's first meeting with Project Advisory Committee will be in August. There will be at least two public workshops to give community members information about the project and to collect input.

Riverfront Park Phase II by Gary Klein

- Construction began in June

Kellogg Good Neighbor Open House Master Plan presented by Dion Shepard

- A handout on the results of the open house
- A diagram of the landscape plan shows how plants and trees will be a visual screen around the pollution control plant.
- No decisions on path lighting

Public Safety Advisory Committee (PSAC) a discussion led by Councilor Hedges on medical marijuana dispensaries in the city of Milwaukie. Everyone was in agreement on the following issues.

- Allow medical marijuana dispensaries in the city of Milwaukie
- Follow the State regulation with no extra restrictions.

NDA Picnic date is Friday, September 12th, 6pm at the Waldorf School

- No meeting on Monday, September 8th
- Dion asked for volunteers to help plan the event
- Ed will cover the music

A discussion on the Reliable Credit request for the NDA support of the new employee parking lot

- Jean Baker made a motion to reject the idea of an employee's parking lot that replaces businesses on Main Street. Mark Gamba seconded the motion. Scott Barber abstained. Motion passed.

Request for reimbursement of expenses

- Jean Baker purchased maps of tax free properties in the city. Dion Shepard made a motion to pay Jean Baker \$46 for her expenses. Ray Bryan seconded the motion. Motion passed.

Meeting adjourned 8:10pm.

Respectively submitted,
Shirley Blalock

From: [Ray Bryan](#)
To: [Kolas, Vera](#)
Subject: Reliable Credit
Date: Wednesday, November 19, 2014 6:37:32 AM

Vera,

I appreciate a business owner who is after the best interests of his/her employees. However I am against the proposed removal of retail stores on Main St, to be replaced with surface parking. Our NDA is also opposed to the plan.

I think a good solution to this problem would be for the city to drop plans to develop the lot across from City Hall, and provide a guarantee of parking for businesses such as Reliable Credit. The parking lot is also beneficial for the Farmers Market, First Friday, and hopefully additional events.

Thank you,

Ray Bryan

From: [Wendy Wagner](#)
To: [Kolas, Vera](#)
Subject: Proposed parking lot beside Reliable Credit
Date: Thursday, November 20, 2014 12:15:17 PM

Dear Vera Kolas--

I was sickened to learn of the plan to demolish the building next door to Reliable Credit. I lived in SE Portland for twelve years and would have never left for the suburbs if I hadn't realized how charming and pedestrian-friendly downtown Milwaukie is. We've lived here for two years and we've been bragging about how the town is a wonderful place to live. Why? Because it has great amenities, like a first-class coffee shop and delicious, one-of-kind eats at Canby Asparagus Farm.

Small businesses that provide great service are what drive a downtown's development and economy. They support a high standard of living for the community's residents. If I wanted to look at another ugly parking lot, I would have moved to Beaverton or Gresham--or even filthy, stinky 82nd Avenue.

I hope there's a way to keep our town from turning into just another faceless, repellent suburb. Perhaps Reliable Credit should take a page from so many local businesses: encourage their employees to take the region's world-class mass transit. After all, they're going to be sitting on top of the brand new MAX line.

Sincerely,

Wendy N. Wagner
4015 SE Monroe St
Writer & Editor
<http://winniewoohoo.com>

From: [Melissa Meadows](#)
To: [Kolas, Vera](#)
Subject: Windhorse and reliable
Date: Thursday, November 20, 2014 11:33:51 AM

Hi Vera,

Thank you for the phone call. I as a Milwaukie Resident am opposed to the tearing down of Historical (in my opinion) buildings that represent Milwaukie, Oregon. Ten parking spaces is ridiculous and economically unsound due to this action will cause many people to hurt financially. So as a long standing resident of Milwaukie, longer than Reliable Credit, I oppose this tearing down of our buildings juts for ten parking spaces. They can take a bus or hey wait the Max train. Save lives save jobs. Milwaukie Rules..

--

Reverend Melissa D. Meadows

From: [Mary Weaver](#)
To: [Kolas, Vera](#)
Subject: downtown parking lot proposal comments
Date: Friday, November 21, 2014 12:10:44 AM
Attachments: [downtown parking lot proposal 11-20-14.docx](#)

Attached are my comments regarding the area at 10605 SE Main Street. I do not have the complete wording of the proposal, but I believe I understand it from the research I have done.

Mary Weaver
Milwaukie resident
503-267-4483



To: Vera Kolias, Associate Planner, City of Milwaukie
 (and to all groups involved in reviewing this proposal)
 From: Mary Weaver, Milwaukie Resident
 Re: comments on the proposal for 10605 SE Main Street
 Date: 11-20-14

I am a frequent visitor to the downtown area and a fan of Wind Horse Coffee & Tea and some of the other local businesses. However, whether I personally visit downtown or particularly like any of the businesses located there is not the issue here. I am very concerned with the future growth, appearance, and "vibe" of downtown Milwaukie. While I understand the downtown parking concerns of Reliable Credit employees, I am also aware there are other solutions available.

Demolishing a building, losing those businesses, and replacing those with an employee parking lot at this site would be a disastrous, character-changing, depressing, uninviting, unproductive action and disrespectful of those who currently enjoy and support our downtown area. It would also result in lost opportunities for those who might have been attracted to this area in the near future, and the loss of that business revenue.

In my opinion, this proposal as I understand it could not possibly fit in ANY logical plan for the future design goals of downtown Milwaukie. While, in general, we all want to allow a property owner to do whatever he or she wants with their property, we all know that doesn't always work out to be reasonable in all circumstances.

I do not have a personal financial connection with any of the parties involved in or affected by this proposal, but I believe as a Milwaukie citizen that all decisions relating to the economic health and positive growth and development of any area of the city - even a seemingly small tax lot such as this one - have an impact on the city as a whole.

I am requesting that you please do all you can to see that this proposal does not become a reality and to encourage the serious discussion by the appropriate parties of a more satisfactory long term parking solution.

Thank you.

Mary Weaver
11656 SE 48th Avenue
Milwaukie, OR 97222

From: [barbara-lee orloff](#)
To: [Kolas, Vera](#)
Subject: Parking VS Buildings and businesses
Date: Thursday, November 20, 2014 1:10:11 PM

As a frequent visitor and buyer of products supplied by the long standing businesses, WindHorse and other, Roger&, how can anyone seriously think a parking lot is the equal to the WindHorse Coffee Shop where for so many years Milwaukians and people from Sunday Market depend on the quality of a cup of coffee and conversation and panni sandwiches toasted just the way we like them. The condo owners across Main do not want to took out on a parking lot!! PLEASE do not build another parking lot on Main Street. Do not allow it Mayor and City Councilors!! Change our codes. Keep Milwaukie thriving with these businesses which we want to keep. Do not let the almighty dollar sway your opinion. Milwaukie is more than cars. It is people who care about their community and shop here and want it to remain viable. Parking lots are like a void.They are empty when the work day is over. Not so the shops which bring people into downtown who appreciate the vibrant changes now occurring. PLEASE do not allow this parking lot and building teardown to happen. It will feel like a death in our community. A vibrant community cannot give precedence to cars over people!! Do not allow a parking lot on Main. It will rid Milaukie of another fine business, WindHorse which deserves better treatment by our fair city by the river.

Barbara-Lee

From: [cassandra](#)
To: [Kolias, Vera](#)
Subject: 10605 SE Main proposal
Date: Monday, November 24, 2014 5:14:55 PM

Hi Vera,

My name is Cassandra and I live in Milwaukie. I was pretty bummed out to hear there are plans to demolish the building that houses Wind Horse Coffee. I know there are a few other businesses in the same bldg but Wind Horse is the place that's a staple in my book.

My chief complaint against it would be why would you tear down something that brings vitality, income and a sense of community to the area for a parking lot? Is it true that's what would be placed in the remains? What a sad prospect. I know the MAX will be done soon and parking might be an issue but there's a big park and ride at Ochoco about a half mile to a mile away (part of my running route) and there's also a pretty big lot there by the bank. So I don't see a parking lot bringing a lot of value or a sense of community to the area. Also there's still construction happening there at River Road where the tracks are being built. Can't a parking lot go over there? That seems closer to the anticipated stop by the high school.

Wind Horse Painted Lady doesn't have hours on Sunday and Spring Creek has a different crowd. I feel like Wind Horse brings a unique cafe feel to the area. I have sat outside during the summer and seen hummingbirds in their bushes. A parking lot won't do that.

Anyway, this disappoints me and seems like a real bonehead move. I like living in Milwaukie and it isn't because of the parking lots.

Thanks for listening, I sure hope this doesn't happen.

Cassandra Caterson 97222
11363 se 27th ave
97222

**CITY OF MILWAUKIE
DESIGN AND LANDMARKS COMMITTEE
NOTES
Milwaukie City Hall
10722 SE Main St
MONDAY, DECEMBER 1, 2014
6:30 PM**

COMMITTEE MEMBERS PRESENT

Sherry Grau, Chair
Val Ballestrem Vice Chair
Adam Argo
James Fossen
Scott Jones

STAFF PRESENT

Denny Egner, Planning Director
Vera Kolias, Associate Planner

MEMBERS ABSENT

None

1.0 Call to Order – Procedural Matters

Chair Grau called the meeting to order at 6:32 p.m. and read the conduct of meeting format into the record.

****Note:** The information presented constitutes summarized minutes only. The meeting audio is available from the Planning Department upon request.*

2.0 Design and Landmarks Committee Minutes

Three sets of meeting minutes were approved by the same motion.

- 2.1 July 7, 2014
- 2.2 October 6, 2014
- 2.3 November 3, 2014

DLC Member Ballestrem moved to approve the July 7, 2014, meeting minutes as presented. There was no second. The minutes were approved unanimously.

3.0 Information Items

Denny Egner, Planning Director, noted that an open house for the Monroe Street Neighborhood Greenway was being held on December 3 at 6:00 p.m. at the Public Safety Building.

4.0 Audience Participation –This is an opportunity for the public to comment on any item not on the agenda. There was none.

5.0 Public Meetings

- 5.1 Summary: Reliable Credit Parking Lot
Applicant/Owner: Tom Sisul, Sisul Engineering/L & B Holzman, LLC
Address: 10605 SE Main St
File: DR-14-07
Staff Person: Vera Kolias, Associate Planner

Chair Grau called the meeting to order and read the conduct of design review meeting format into the record.

Vera Kolias, Associate Planner, provided an overview of the application and staff recommendation via PowerPoint presentation.

- The applicant proposed to construct a new surface parking lot on the site of a commercial building.
- Because the development was located within the Downtown Commercial zone, it was subject to Type III Downtown Design Review, which required a recommendation from the DLC to the Planning Commission.
- The applicants had proposed utilitarian light fixtures, rather than the ornamental style recommended by the Downtown Design Guidelines, and the usage of landscaping to define the street edge.
- The applicant had provided a revised proposal at the meeting, which had been distributed to the Committee. This proposal included a low seat wall along Main St. and turned the corner along Scott St.
- Staff felt that landscaping did not sufficiently address the pedestrian environment, and recommended a condition of approval requiring the provision of a structural storefront facade or seat wall along the Main St frontage in order to meet the pedestrian emphasis guidelines.
- Staff recommended approval with recommended findings and conditions of approval.

The Committee asked questions about the application.

- "Substantial" was an interpretation for the Committee to make, but staff's recommendations were that a wall along Main St would provide the street edge. The wall had not been designed and the design of the wall would need to return to the Committee for review.

Chair Grau called for applicant testimony.

Lee Holzman, Reliable Credit, 10690 SE McLoughlin Blvd, Milwaukie, provided an overview of the reason for the request.

- Reliable Credit had moved to its current location at 10633 SE Main St in 2001. Parking had been a concern, and he had bought the commercial building at 10605 SE Main St in 2010 in the event that additional employee parking was needed.
- In the past year, an employee had been hit by a car while crossing Harrison St to the City parking lot. This had motivated him to review Reliable Credit's parking situation.
- There was also concern about losing employee parking spaces at the City Hall parking lot when or if the Texaco Site redeveloped. He was trying to be proactive and address the parking situation now.

Tom Sisul, Sisul Engineering, 375 Portland Ave, Gladstone, provided additional information about the parking lot design.

- The staff report had indicated that the lot was 15,000 sf, but that number included both the Reliable Credit lot and the commercial building lot.

- The proposed parking lot was 15 stalls rather than the 13 stated in the staff report. 13 is the net number of new parking spaces.
- The applicant did not have significant opposition to the recommended conditions of approval. Staff preferred the use of ornamental lights rather than "shoe box" lights, and the applicant did not have any significant opposition to that request.
- He reviewed the revised proposal that had been provided to the Committee, which included 4 ornamental ("acorn") lights and a short wall of seating height along Main St and wrap around to Scott St. The seat wall was preferable to a fake building facade, because the seat wall provided visibility to and from the proposed parking lot. There had been some questions about the large tree on the site, which was expected to remain.

DLC Member Jones asked which materials would be used for the seat wall. **Mr. Sisul** noted that the wall would be constructed of brick or block in a light gray color, which was chosen because it was neutral.

Mr. Ballestrem asked how the parking lot enhanced the sense of place in downtown Milwaukie. **Mr. Sisul** noted that the parking facility wouldn't have the same character as the existing building. It was an allowed use in the zone, and the application was being submitted at this time to ensure that Mr. Holzman had the opportunity. The lot was proposed to provide convenience and safety for the employees. It would be landscaped and bring greenery to the corner, where there was currently none. The low wall would bring a structural element and the proposed acorn street lights would provide some sense of place.

Mr. Fossen asked how the current parking lot was used. **Mr. Holzman** stated that the current parking lot was used during the Farmers Market. When the Texaco Site property was developed, he would consider allowing the Farmers Market to locate on the existing and proposed parking lots.

Chair Grau asked how many employees and parking spaces Reliable Credit currently had. **Mr. Holzman** stated that there were currently 50 employees and 21 parking spaces. **Chair Grau** asked if there might be a diminished need for parking with the downtown light rail station. **Mr. Holzman** thought that the light rail would help but would not fully address the problem.

DLC Member Argo asked if Mr. Holzman had a sense of how his employees traveled to work, or if they had done any type of study of how people traveled to work. **Mr. Holzman** stated that the employee that had been struck by a car was walking to his car after work when he was hit while crossing Harrison St. He did not know how many people drove as opposed to riding a bike to work. He offered to find that information.

Mr. Ballestrem asked for further clarification about a statement that Reliable Credit would not be able to building a surface parking lot in the future. **Mr. Sisul** explained that the City was currently reviewing code revisions for downtown Milwaukie and he assumed that a version of the changes would be adopted. He understood that while surface parking lots were currently allowed along Main St north of Harrison St, the proposed regulations would prohibit surface parking lots along Main Street north of Harrison St. Mr. Holzman wished to preserve the right to build a surface parking lot on the site, and this was the only action he could control.

Mr. Jones asked whether a best use analysis or needs assessment had been conducted for the site. **Mr. Sisul** stated that no other options had been discussed. The building was currently occupied by several small businesses and the proposal was to remove the building and install a parking lot.

Chair Grau called for testimony in support of the application. There was none.

Chair Grau called for neutral testimony.

Denise Baker, 10606 SE Main St, Milwaukie: She supported the concept of a street wall similar to the New Market Theater in Portland rather than a street wall. She agreed that the pedestrian value and historic value would be changed with the removal of the building.

Charles Maes, 10605 SE Main St, Milwaukie: Owner of Casa de Tamales and tenant of the building. He respected Mr. Holzman and what he wanted to do with his property. He asked the Committee to help Mr. Holzman find another location for parking. He did not feel that demolishing the building would help downtown. He asked for assistance with saving the building.

Geoffrey Janke, 1237 SE River Forest Ln, Oak Grove: Spends quite a bit of time in downtown Milwaukie. Questions whether a parking lot is the best use of the space. Would like to see additional discussion about best use of the street and whether the proposal would actually increase the safety of the employees.

Chair Grau called for testimony in opposition to the application.

Christie Schaffer, 10606 SE Main St, Milwaukie: Has lived in Milwaukie since the early '70s. Doesn't want another parking lot on Main St. The owner should know the situation of employees before putting in a parking lot. Would like to keep it as a part of Main St. Asked the City to work with the applicant to find another way.

Ceci Denovo, 2615 SE Willard St, Milwaukie: Has lived in Milwaukie since 2005. Very opposed to the project, feels that it is against the goals of the city. Rather than building business it destroys sense of place and eliminates businesses that provide jobs and bring people in. It is extremely unjust that business owners are losing income and employees as well. It is also very inconvenient for Milwaukie residents who are patronizing these businesses. The recommendation is to mimic storefronts, but we should keep a real storefront with businesses behind them. It does not seem that the applicant is clear about what the employee parking needs are.

Alicia Hamilton, 11921 SE 19th Ave, Milwaukie: One of the organizers of First Friday event, and has put a lot of volunteer time and effort into making city better. Respects and understands the needs of Mr. Holzman and Reliable Credit. It is devastating to lose 5 businesses for a parking lot. Implored the City and property owner as well as future builders and developers to work together to find solutions to parking so that a businesses don't have to build them on their own. She understood that a surface parking lot was allowed by code, and asked the Committee to consider several things: mimicking storefronts does create a sense of place; would look at lighting, acorn style is consistent but if living across the street might prefer downward lighting; consider providing a mural on the back wall and Mr. Holzman might consider donating to the Milwaukie mural program.

Val Hubbard, 10669 SE 21st Ave, Milwaukie: Loves walking downtown. The small businesses downtown really enhance her life in Milwaukie. It saddens her to think that this could become a parking lot. Concerned about person getting hit by the car, but he could have been hit going to lunch rather than to his car. Hope that they care about Milwaukie as much as she cares about Milwaukie. We are on cusp of making the downtown a great place, but the proposed design is not architecturally interesting. If the project moves forward would like it to beautify the City.

Chair Grau called for additional questions from the Committee.

Chair Grau asked if Reliable Credit had parking incentives for the use of transit. **Mr. Holzman** stated that there was no transit incentive. He addressed comments about how he did not know

the employee need for parking. He had asked how many employees purchased parking from the City at the Texaco site. Reliable Credit employees purchase more than the net 13 spaces that would be provided in the new parking lot.

Chair Grau asked if members of the audience could ask questions of the Committee or the applicant. **Mr. Egner** stated that it was the Committee's choice, and any statements should be directed to the Chair.

Mr. Maes noted that Mr. Holzman could buy transit passes for 50 employees and it would cost less than building a parking lot or paying for 50% of the parking passes for employees.

Mr. Egner noted that he was not sure it was proper to restart the hearing by allowing additional comments and testimony.

Chair Grau noted that the issue at hand was not the removal of the building, but whether the parking lot design met the Downtown Design Guidelines.

Ms. Baker asked whether the City had considered working with Mr. Holzman on construction of a two-story building that could accommodate commercial spaces and parking, and whether the City had followed up on urban renewal funds that could help with that type of project.

Unknown female in the audience noted that the applicant had stated that the parking lot would not be permitted in the future due to proposed zoning updates. She asked if there was a possibility that the parking lot permission could be "grandfathered in" so that the applicant could build a parking lot in the future.

Mr. Egner responded to the questions.

- The Community Development Director had met with Mr. Holzman several times and the City was still talking with them.
- The City did not currently have an urban renewal district, though it was part of the Moving Forward Milwaukie discussion.
- If approval is given for the parking lot the applicant has two years to do so and can also offer an extension to the approval, and they are under no obligation to construct the parking lot right away.

Tim McMenamin, 13063 SE Capistrano Ct, Milwaukie, 97222: Asked if there were plans for development of the City parking lot across from Reliable Credit (the Texaco Site). **Mr. Egner** noted that the site had been identified as an opportunity site through the Moving Forward Milwaukie project, and the City was looking at joint development opportunities. The City had worked with Metro several years ago to develop the site but the economy had collapsed and the project did not move forward. The City also needed to revisit a downtown parking plan in the event that property developed. **Mr. McMenamin** proposed an alternative option for the site which would be to create a covered Farmers Market and add a second level parking structure.

Mr. Janke asked if the project would include ADA spaces or promote alternative transportation methods.

Chair Grau called for applicant rebuttal.

Mr. Sisul addressed the testimony and comments.

- The proposed parking lot would be on the same lot, so no one would need to cross the street. It would also be closer than other parking lots.
- Agreed that down facing street lights would protect adjacent residences from light glare.

- The building to the rear of the parking lot is not owned by Mr. Holzman, so the owner of the building would need to install a mural.
- The site was too small for a two story facility including parking due to ramps. Would not work well for parking or as a retail area.
- The City's lot is large enough to construct a two story parking facility with street-level storefronts.
- Sometimes the rules change and you aren't able to get an extension to the approval because the rules could change. They should exercise caution and not assume that an extension would be granted.
- An extra ADA stall would be added in front of the main entry to meet ADA requirements triggered by the parking expansion.

Mr. Holzman noted that he had put a lot of money into making the Reliable Credit building look nice. The parking lot was not a financial venture – he was comfortable spending the money to make it to look nice.

Chair Grau closed public testimony portion of the meeting.

The Committee discussed the proposal.

- Agreed that the site should be visually interesting.
- Agreed that a hybridized approach to the site lighting was appropriate: shoe box lighting was appropriate along the western edge of the parking lot, and acorn lighting was appropriate along the Main St edge.
- Supported the low wall concept, but a storefront façade would better meet the Downtown Design Guidelines.
- Felt that the lighting along Main St should be shielded from the residences across the street in some manner.
- Determined that, as conditioned and with modified conditions of approval, the application met the approval criteria.

Mr. Ballestrem moved to recommend approval of the application with modified findings allowing shoe box lighting along the western edge of the parking lot and requesting a façade rather than a seat wall along Main St. **Mr. Argo** requested that the motion be amended to require two or three alternative concepts for DLC approval. **Mr. Ballestrem** modified the motion to include at least two storefront designs be submitted to the DLC for review. **Mr. Fossen** seconded the motion, which was approved unanimously.

6.0 Worksession Items

There were none.

7.0 Other Business/Updates

There were none.

8.0 Design and Landmarks Committee Discussion Items

There were none.

9.0 Forecast for Future Meetings:

CITY OF MILWAUKIE DESIGN AND LANDMARKS COMMITTEE
Minutes of December 1, 2014
Page 7

January 5, 2014	1. Cancelled
February 2, 2014	1. TBD

Meeting adjourned at approximately 8:08 p.m.

Respectfully submitted,

Li Alligood, Senior Planner

Sherry Grau, Chair



MILWAUKIE

Dogwood City of the West

To: Planning Commission

Through: Dennis Egner, Planning Director

From: Li Alligood, Senior Planner

Date: December 2, 2014, for December 9, 2014, Worksession

Subject: Moving Forward Milwaukie Briefing #11: Draft Amendments to the Downtown & Riverfront Land Use Framework Plan

ACTION REQUESTED

None. This is a briefing for discussion only. This is the eleventh in a series of project briefings to the Planning Commission. Staff is seeking direction regarding regulatory incentives for green building, and feedback about draft amendments to the *Downtown and Riverfront Land Use Framework Plan*.

BACKGROUND INFORMATION

A. Downtown Vision

The Downtown and Riverfront Land Use Framework Plan ("Framework Plan"), which is the adopted vision for downtown, describes 3 "fundamental concepts":

1. Anchors and Attractors – businesses at either end of Main Street that draw substantial pedestrian traffic and provide additional customers for downtown businesses.
2. Main Street as a healthy retail street – establishing a "lively storefront retail character with a pedestrian emphasis and 24-hour use."
3. Connecting downtown to the river – signalized pedestrian connections to the riverfront and development of Riverfront Park.

The Downtown Vision, as described in the Framework Plan, was adopted in 2000 and was subsequently reaffirmed through the *Fresh Look Milwaukie: Downtown Road Map* project in spring 2013. The proposed amendments would update the Framework Plan to reflect changes since 2000, including: plans that have been adopted (the Transportation System Plan, Riverfront Park Master Plan, and South Downtown Concept Plan); projects that have been completed; and projects that are currently underway. The updated Framework Plan is intended to reflect the current direction of downtown Milwaukie.

B. History of Prior Actions and Discussions

- **November 13, 2014:** Staff provided an overview of the proposed revisions to the downtown design review procedures for development in downtown Milwaukie to the Commission and the Design and Landmarks Committee at a joint worksession. The Commission determined that any height or FAR bonuses would be cumulative; directed staff to incorporate development incentives for the provision of additional open space; requested additional information regarding potential incentives for green building; and agreed with the proposed streamlined approach to downtown design review.
- **October 28, 2014:** Staff provided an overview of the proposed revisions to the design standards in downtown Milwaukie. The Commission requested additional information regarding potential incentives for green building and/or additional open space.
- **October 14, 2014:** Staff provided an overview of proposed revisions to the development standards in downtown Milwaukie. The Commission discussed building height, off-street parking requirements, build-to lines, and ground floor window requirements.
- **September 23, 2014:** At a joint worksession with City Council, Matt Arnold of SERA Architects provided a presentation and led a discussion about downtown development and design standards and consideration for downtown Milwaukie.
- **September 9, 2014:** Staff provided an overview of potential draft code amendments for downtown Milwaukie, specifically related to downtown use standards, and led a walking tour of the North Main Village development.

KEY DISCUSSION ITEMS**A. Green building incentives**

At the October 28 worksession, the Planning Commission requested additional information about how the City could incentivize design that included green building (such as LEED certification) and/or the provision of additional open space. At the November 13 worksession, the Planning Commission requested additional information about green building incentives in other, similarly-sized communities, before making a decision about whether to incorporate development incentives for green building into the draft code amendment package.

Staff researched other small cities in Oregon and the types of the green building incentives they provide. Only Ashland, OR (pop. 20,713), provides regulatory incentives in the form of bonus residential densities for LEED or Earth Advantage certification. See Attachment 1. According to staff, the developer must submit a certification with the building permit submittal that the plans meet LEED or Earth Advantage requirements. A third party reviewer evaluates the plans to confirm compliance.

City staff was not able to identify any other small cities in Oregon with regulatory incentive programs. Most programs take the form of expedited permit processing, fee waivers or reductions, or other process-based incentives.

B. Downtown and Riverfront Land Use Framework Plan

The Downtown and Riverfront Land Use Framework Plan ("Framework Plan") was adopted in 2000 and reflected the community's vision for downtown, as well as the infrastructure and development projects that would implement the vision. A key component of the Moving Forward Milwaukie project is a "refresh" of the Framework Plan to reflect the revised policies, completed projects, and new projects that have been adopted or implemented since 2000.

See Attachment 2 for the adopted Framework Plan; see Attachment 3 for commentary describing the proposed amendments, which are shown in underline/strikeout; and see Attachment 4 for the "clean" version of the document.

C. Discussion Questions

Staff is seeking Commission feedback about the following proposals.

1. Which, if any, potential bonus densities (FAR, height) should be included in the draft code amendments for green buildings?
2. If so:
 - a. Should various levels of incentives be tied to various levels of certification?
3. Are there any questions or concerns about the proposed revisions to the Framework Plan?

Next Steps

The first hearing for the draft downtown plan and code amendments is scheduled for Tuesday, January 13. Staff is incorporating feedback from the Planning Commission, Design and Landmarks Committee, and the public into a revised review draft of the amendments, which will be available on December 12.

Staff requests Planning Commission feedback about whether additional worksessions on the draft amendments are desired before beginning the public hearing process.

ATTACHMENTS

Attachments are provided as indicated by the checked boxes. All material is available for viewing upon request.

	PC Packet	Public Copies	E- Packet
1. City of Ashland Green Building Incentives	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
2. Downtown and Riverfront Land Use Framework Plan	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
3. Underline/Strikeout Amendments with Commentary	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
4. Clean version, as proposed to be amended	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

Key:

PC Packet = paper materials provided to Planning Commission 7 days prior to the meeting.

Public Copies = paper copies of the packet available for review at City facilities and at the Planning Commission meeting.

E-Packet = packet materials available online at <http://www.milwaukieoregon.gov/planning/planning-commission-117>.

18.88.040 Performance Standards for Residential Developments**A. Base Densities.**

1. The density of the development shall not exceed the density established by this Section. The density shall be computed by dividing the total number of dwelling units by the acreage of the project, including land dedicated to the public. Fractional portions of the final answer, after bonus point calculations, shall not apply towards the total density. Base density for zoning districts within the City of Ashland shall be as follows:

WR and RR zone--1 divided by the minimum lot size expressed in acres, times 0.60 determines the dwelling units per acre.

$$\text{WR-2} = 0.30 \text{ du/acre}$$

$$\text{WR-2.5} = 0.24 \text{ du/acre}$$

$$\text{WR-5} = 0.12 \text{ du/acre}$$

$$\text{WR-10} = 0.06 \text{ du/acre}$$

$$\text{WR-20} = 0.03 \text{ du/acre}$$

$$\text{RR-1} = 0.60 \text{ du/acre}$$

$$\text{RR-.5} = 1.2 \text{ du/acre}$$

Single Family Zones

$$\text{R-1-10} = 2.40 \text{ du/acre}$$

$$\text{R-1-7.5} = 3.60 \text{ du/acre}$$

$$\text{R-1-5} = 4.50 \text{ du/acre}$$

$$\text{R-1-3.5} = 7.2 \text{ du/acre}$$

Multi-Family Zones

$$\text{R-2} = 13.5 \text{ du/acre}$$

$$\text{R-3} = 20 \text{ du/acre}$$

2. All developments with a base density of 10 units or greater shall be required to provide a minimum of 5% of the total lot area in Open Space that is not subject to bonus point calculations. Bonus shall be awarded only to that Open Space area in excess of the 5% required for developments of 10 units or greater. Open Space shall be optional for all developments of less than 10 units.

B. Bonus Point Calculations.

1. The permitted base density shall be increased by the percentage gained through bonus points. In no case shall the density exceed that allowed under the Comprehensive Plan.

2. The maximum bonus permitted shall be 60%. (Ord. 2669, 1992)

3. The following bonuses shall be awarded:

a. Conservation Housing – 100% of the homes or residential units approved for development, after bonus point calculations, shall meet the minimum requirements for certification as a Earth Advantage home, as approved by the Ashland Conservation Division under the City' s Earth Advantage program as adopted by resolution 2006-06 maximum 15% bonus. (Ord 2923; S3 2006)

b. Provision of common open space.

1. **Purpose.** Common open spaces may be provided in the form of natural areas, wetlands, playgrounds, active or passive recreational areas, and similar areas in common ownership. All areas set aside for common space may be counted for base density, unless otherwise excluded by the Land-use Ordinance. However, for the purposes of awarding density bonus points, the Planning Commission shall consider whether or not the common open space is a significant amenity to project residents, and whether project residents will realistically interact with the open space on a day-to-day basis. The purpose of the density bonus for common open space is to permit areas which could otherwise be developed, or sold as individual lots, to be retained in their natural state or to be developed as a recreational amenity. It is not the purpose of this provision to permit density bonuses for incidental open spaces which have no realistic use by project residents on a day-to-day basis.

2. **Standard.** Developments of 10 units or greater shall provide 5% of the total development area in Open Space. No bonus points shall be awarded for this Open Space. For developments of less than 10 units which provide more than 2% of the project area for common open space, or for developments of 10 units or greater which provide greater than 5% open space, a 1% bonus shall be awarded for each 1% of the total project area in common open space--maximum 10% bonus.

c. Provision of major recreational facilities.

1. **Purpose.** Points may be awarded for the provision of major recreational facilities such as tennis courts, swimming pools, playgrounds, or similar facilities.

2. **Standard.** For each percent (1%) of total project cost devoted to recreational facilities, a 6% density bonus may be awarded up to a

maximum of 10% bonus. Total project cost shall be defined as the estimated sale price or value of each residential unit times the total number of units in the project. Estimated value shall include the total market value for the structure and land. The cost of the recreational facility shall be prepared by a qualified architect or engineer using current costs of recreational facilities--maximum bonus 10%.

d. Affordable Housing - for every percent of units that are affordable, an equivalent percentage of density bonus shall be allowed. Affordable Housing bonus shall be for residential units that are affordable for moderate income persons in accord with the standards established by resolution of the Ashland City Council and guaranteed affordable through procedures contained in said resolution. Maximum bonus of 35%.(Ord. 2450 1988; Ord. 2630 S6, 1991; Ord. 2669, 1992)

MILWAUKIE DOWNTOWN AND RIVERFRONT LAND USE FRAMEWORK PLAN

**ANCILLARY DOCUMENT
TO
MILWAUKIE COMPREHENSIVE PLAN**



*Adopted September 19, 2000
(Ordinance No. 1880)*

MILWAUKIE, OREGON



September 19, 2000

Dear Colleagues:

During 1999 and early 2000, the community met to develop a new vision for downtown Milwaukie and the Willamette Riverfront. The attached Milwaukie Downtown Plan, the result of creative thinking, hard work and dedication from Milwaukie residents and City staff, will ensure that new development and redevelopment are consistent with that vision.

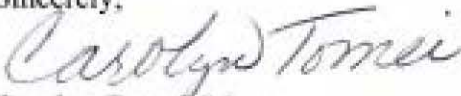
City officials, community leaders and area businesses have known for a long time that Milwaukie's historic downtown, nestled at the edge of the Willamette River, can and should be a vital center for the community's economic and social activity. The Milwaukie Downtown and Riverfront Plan identify policies to make downtown Milwaukie:

- a livable community where downtown residents can walk to work and shop conveniently;
- a thriving business center; and
- a destination for family activities, featuring salmon-friendly natural areas and a riverfront park.

The heart of our city is the downtown. A thriving downtown will be an integral part of making Milwaukie a more sustainable, livable city. As a longtime participant in the public process, I am confident that with this plan, a commitment from the community, and dedicated partnerships, our downtown will be reinvigorated.

In appreciation to the hundreds of Milwaukie citizens and numerous staff members who worked to make this plan a reality, we dedicate the Milwaukie Downtown and Riverfront Plan to the residents of Milwaukie.

Sincerely,


Carolyn Tomei, Mayor

MILWAUKIE CITY HALL
10722 SE MAIN STREET
MILWAUKIE, OREGON 97222
PHONE: (503) 786-7555 • FAX: (503) 652-4433

Acknowledgements

The Citizens of Milwaukie

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Carolyn Tomei, Mayor
Mary King
Larry Lancaster
Jeff Marshall
Brian Newman

Planning Commission

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Judith Borden
Barbara Cartmill
Donald Hammang
Mike Miller
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Downtown and Riverfront Land Use Framework Plan

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Land Use Framework

The Milwaukie Downtown and Riverfront Land Use Framework Plan represents a major opportunity to reinvigorate downtown Milwaukie. With aggressive and focused efforts from both the public and private sectors over the coming years, downtown Milwaukie will be revitalized. This Plan outlines the components necessary to make such a vision of Milwaukie a reality. It foremost builds upon existing assets, capitalizes upon significant existing uses, and features the natural environment. It also sets out a realistic agenda and implementation program that the city and private development can follow.

What This Framework Does

The Land Use Framework is a vision of what can occur in the downtown and riverfront area. It has been tailored to meet Milwaukie's social, economic and development goals and policies while responding to market conditions and the City's overall vision for future growth.

This document serves as an ancillary document to the Milwaukie Comprehensive Plan. On its own, this Framework is not a regulatory document. Any part of the Framework that is intended to have a binding effect will have to be adopted as part of, or pursuant to, a code to have regulatory effect. The Land Use Framework suggests changes to the existing Town Center Plan Subarea 1 of the Comprehensive Plan.

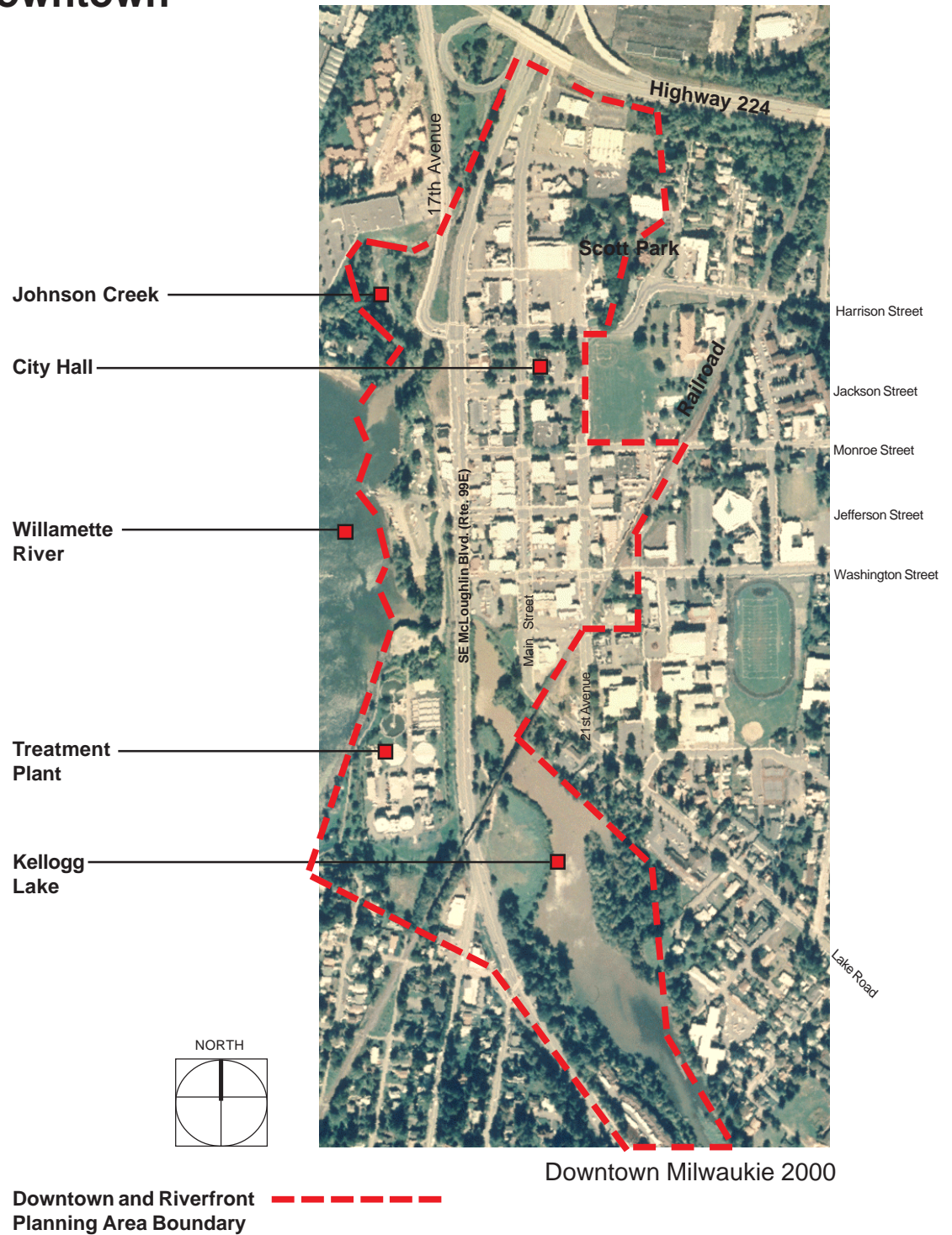
This Framework establishes and guides the development of publicly and privately owned parcels of land, and outlines specific land uses. Nothing in this framework authorizes public use of private property. The Zoning Ordinance provisions regarding permissible uses prevail over any inconsistent provision in this Downtown and Riverfront Land Use Framework.

This Land Use Framework allows existing businesses to remain as long as their owners wish. All privately owned property designated as a public use in this Framework - for example, parks or trails - will continue to be in a "private ownership" zoning category, and will not be rezoned for public use until it is acquired by a public entity.

If an existing business does not conform to the new planning area's allowable uses and development regulations, it will be allowed to continue to operate, as well as to expand and rebuild, in accordance with the Downtown Design Standard Threshold Regulations of the City of Milwaukie's Zoning Ordinance.

Land Use Framework

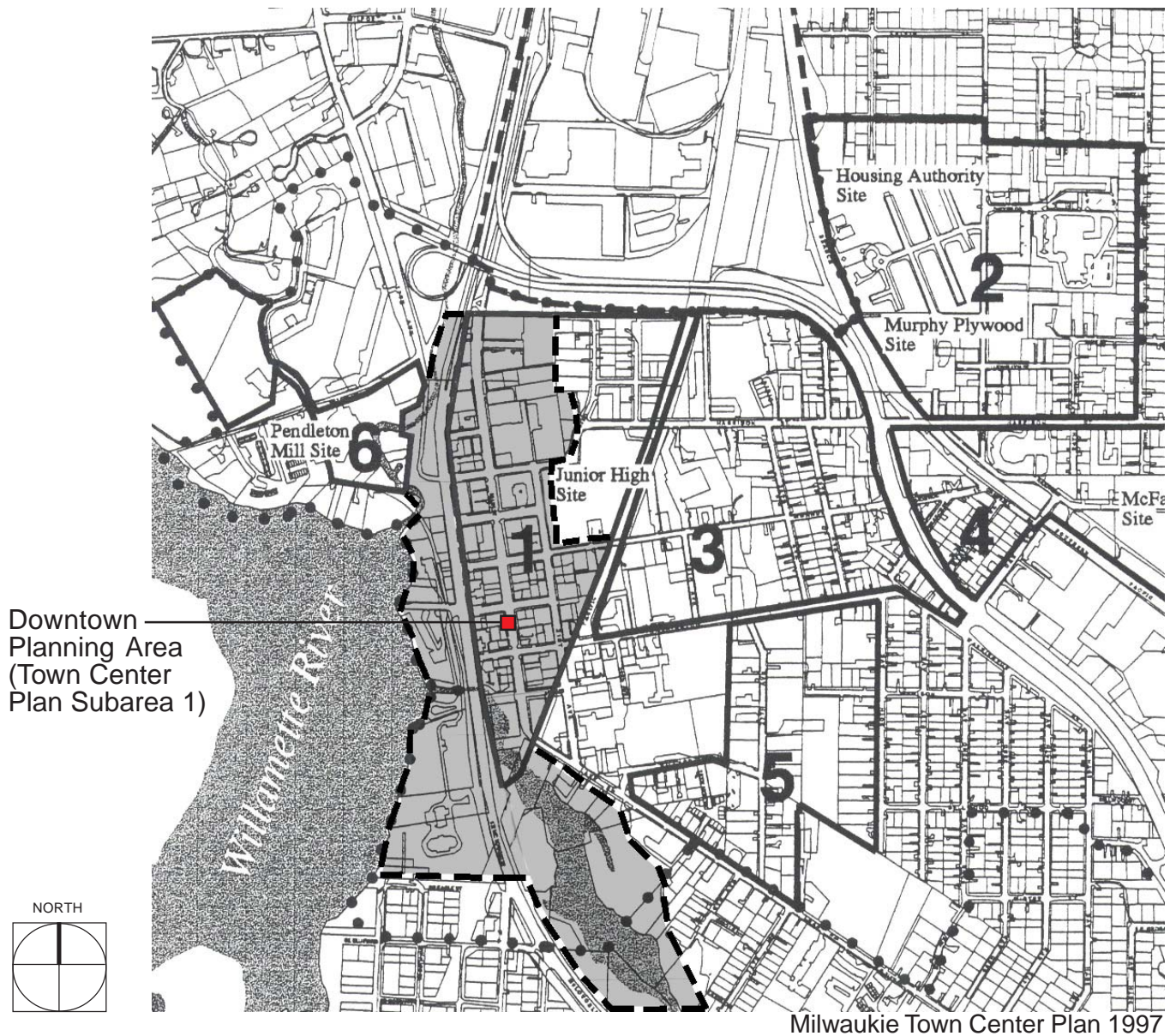
1.1 Aerial Photograph of Existing Downtown



Land Use Framework

1.2 Downtown Planning Area Map

Subarea 1 of the Town Center Master Plan

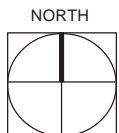


Land Use Framework

1.3 Illustrative Plan

Land Uses

	Retail/Mixed Use
	Housing
	Office
	Civic
	Arts/Entertainment
	Hotel
	Parking Structure
	Recreation and Opens Space



Land Use Framework

1.4 Framework Elements

The Milwaukie Downtown and Riverfront Land Use Framework recognizes and builds upon the rich character and history of our town. It celebrates the diversity and spirit of the people and places of Milwaukie, and reinforces those special qualities.

City officials, community leaders and area businesses have known for a long time that Milwaukie's historic downtown, nestled at the edge of the Willamette River, can and should be a vital center for the community's economic and social activity. Starting in 1999, the City of Milwaukie began a process to develop a concept for downtown Milwaukie that reflects the vision of the people who live and work there. During a number of community design forums, citizens have tested various aspects of the plan, with local residents ranking their priorities for the downtown area.

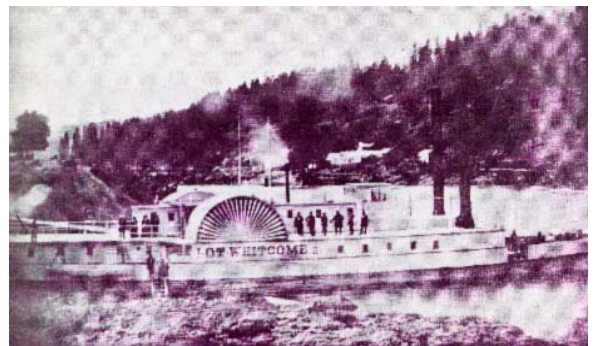
This Land Use Framework represents the leadership of a skilled volunteer Riverfront Board and the input of the more than 2000 community members who have attended meetings, returned surveys, provided focus and ideas, and directed the plan.

The Recipe that Works

This Land Use Framework represents a process derived from a recipe of fundamental concepts that has worked successfully in other cities, equal in size to Milwaukie and with similar challenges. In those cities, the public and private sectors worked together to make revitalization happen. They developed a plan, and subsequently residents and government officials did what it took to make their plans work. Their efforts paid off. This Land Use Framework will do the same for Milwaukie.



Downtown view from Willamette River



Lot Whitcomb, 1850



Historic Masonic Lodge

Fundamental Concepts

Milwaukie's unique character is at the heart of the Downtown and Riverfront Framework. The Framework reconnects Milwaukie to the Willamette River, knitting together the seam of McLoughlin Boulevard. It creates the new Riverfront Park as the city's "living room." And it calls for revitalizing historic buildings while designing new structures to harmonize with the town's historic character.

Anchors and attractors are used to build upon existing resources, and to strengthen the Main Street "retail armature." Thus the Framework is a blueprint to make Milwaukie and its downtown a vital, livable and sustainable community.

Amenities and Open Spaces

Milwaukie is fortunate to have a setting that inspires its citizens, that offers history, beauty and vitality. The Willamette River, Johnson Creek, and Kellogg Creek all provide beautiful natural borders to the downtown area. The Milwaukie Downtown and Riverfront Framework capitalizes on these natural resources, by restoring the creeks and connecting the river to the historic blocks of downtown. Thus both the town and the natural areas are reinvigorated.

Implementation

This Framework coordinates with Milwaukie's existing planning efforts and regulations. The Milwaukie Comprehensive Plan provides the guiding policies to manage the city's physical elements. Ancillary planning documents may be attached to the Comprehensive Plan. These include the Milwaukie Vision Statement of 1995 and the Town Center Master Plan of 1997. The latter responds to Milwaukie's designation as one of Metro's 2040 Growth Concept Town Centers. The Downtown and Riverfront Framework is a conceptual document. Therefore, various sections may be adopted as part of, or referenced by the existing Milwaukie Zoning Ordinance, Subdivision Ordinance, or Municipal Code to be effective as regulatory document.

The Downtown and Riverfront Framework responds to environmental policies including Metro's Title 3 Requirements and Federal Endangered Species Act.

Phasing and Financing Strategy

The Framework makes this bold statement: that investing in the future of Milwaukie makes sense. Investment offers employment and opportunity for all the citizens of Milwaukie; it enriches the town literally and figuratively. Smart public improvements stimulate substantial private investment. Thus, investing today in a better quality of life for downtown Milwaukie makes sense for the whole town, for years to come.

Land Use Framework

1.5 Our Guiding Principles - A Touchstone

In 1999, the City of Milwaukie began a process to develop its vision of the downtown and riverfront. Throughout this community effort, the Framework was developed in accordance with and responded to the following guiding principles:

Creating a livable community:

- Provide for residents, workers and visitors alike.
- Provide for people of all ages, cultures, ethnic groups and incomes.
- Provide cultural arts and entertainment facilities.
- Provide significant open spaces and connections to the riverfront.
- Provide for specific “programmatic” requirements, such as parking or visibility from major roadways.

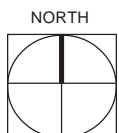
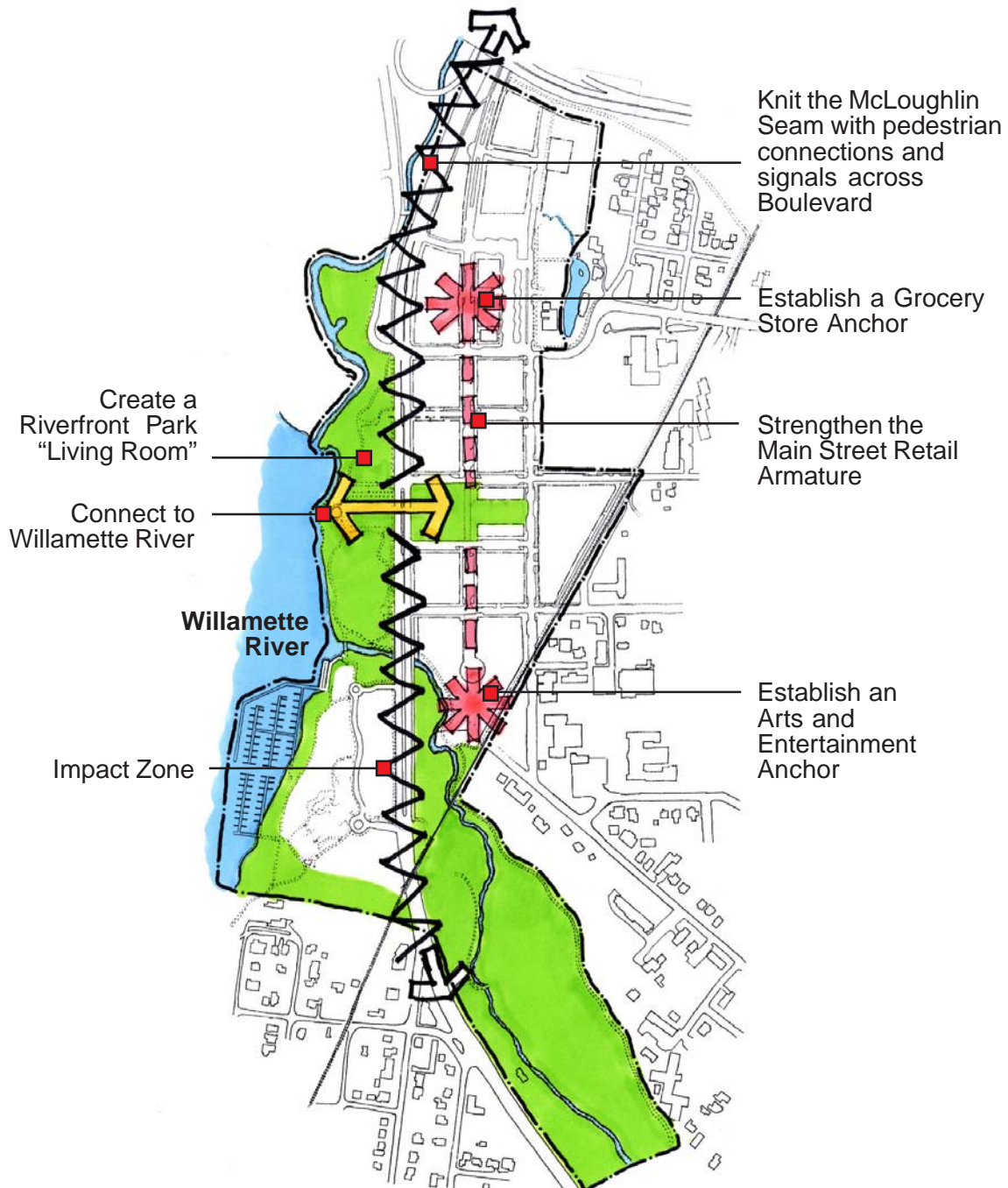
Ensuring economic success:

- Efficiently maximize current investment in infrastructure.
- Spur further private investment.
- Recognize and respond to the current marketplace.
- Establish a strategy for capturing unrealized market niches.
- Complement, protect and promote the continued growth and vitality of current businesses.



Land Use Framework

1.6 Fundamental Concepts Map



Land Use Framework

1.7 Fundamental Concepts

Anchors and Attractors

The keystone to building a successful downtown is to build upon existing resources - the quality stores and offices that we already have - and supplement these with anchors and attractors - places used by hundreds of people on a daily basis. A grocery store, for example, will generate considerable foot traffic, which will in turn provide additional customers for downtown businesses.

The framework includes key elements which will be necessary to achieve these goals. New “anchor” uses are as follows:

- Bus transit center.
- Grocery store across Main Street from the transit center.
- Arts, entertainment and office “campus” of buildings at the southern end of Main Street, including a graphics-oriented higher education facility.

The Main Street “Retail Armature”

Reactivating Main Street is a major focus - re-establishing and strengthening a lively storefront retail character with a pedestrian emphasis and 24-hour use.

The framework establishes an environment in which people can shop, work, live and socialize along Main Street. It addresses and repairs the fundamental problems that have drained downtown of its vitality. The Framework suggests adding to or filling in blocks with new uses and in some instances tearing down buildings and starting over again.

In the four blocks between these two anchors, the fabric of ground floor retail establishments will create a lively flow of pedestrian activity. The Main Street retail armature ensures that a healthy retail street includes:

- Retail on both sides of the street.
- Continuous retail facades with no interruptions.
- On-street parking in front of retail.
- Anchor retail at both ends of Main Street.
- Retail on all four corners of intersections.
- A pedestrian loop.
- Safe, signalized pedestrian crossings.
- Pedestrian-friendly amenities - wide sidewalks, landscaping, benches.

This north-south flow of activity will be further enlivened where it intersects with the new Main Street Plaza - in the block between Monroe and Jefferson Streets and leading directly to the Willamette River and the new Riverfront Park.

Connecting to the River

The new Riverfront Park will be the location for special events such as “Festival Daze,” holiday celebrations and community assemblies. At its southern end will be a new hotel, adjacent to the new public marina and rowing facility.

Land Use Framework

1.8 Land Use Framework

Planning Areas:



Commercial



Housing



Storefront Main Street



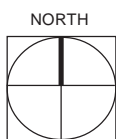
Arts/Entertainment/Office



Public Park



Hotel



Land Use Framework

1.9 Key Land Use Features

Revitalizing Main Street and downtown:

- Main Street improvements - shops, services, and family wage jobs
- Save Landmarks - to preserve history and heritage
- New Grocery Store - to anchor Main Street
- New Tri-Met Bus Transit Center
- New Arts and Entertainment Campus



Revitalizing Main Street

Reconnecting to the River:

- New McLoughlin Bridge - to knit the seam between the downtown and the river
- New Riverfront Park - the City's living room
- New Public Marina and restaurant
- New Riverfront hotel - to replace the water treatment plant



Reconnecting to the River

Restoring Natural Areas and Parks:

- Downtown stream - divert a portion of Spring Creek into downtown park
- Restore Spring Creek
- Restore Johnson Creek
- Restore Kellogg Creek



Restoring Natural Areas - Spring Creek

Providing Quality Housing:

- To the North, townhomes and apartments engaging new parks, near Spring Creek and trail to Spring Water Corridor
- To the South, townhomes and apartments along landscaped creek and Rail Trail.



Providing Quality Housing

Land Use Framework

1.10 Land Use Descriptions

Downtown Milwaukie includes six established and emerging planning areas, each with distinctive physical characteristics and varying uses. While they share a singular overall area - the downtown of Milwaukie - they serve various social, cultural, and economic roles. The goal of the Downtown and Riverfront Plan is to secure a future which binds all of these existing and potential areas into a coherent downtown while enabling each individual area to maintain or develop a distinctive identity.



Bus Transit Center



Storefront Main Street



Apartments - North Housing

Land Use Framework

1.11 Housing North Area



Character:

Multiple-family residences: ownership/condominiums and rental (including townhouses and apartments).

Approximate Area:

10 acres

Proposed Use:

25 townhouse units (target)

225 units apts/condominiums (target)

Total units:

250 (target)

Land Use Framework

1.12 Housing South Area



Character:

Multiple-family residence types: rental apartments or condominiums.

Approximate Area: 2 acres

Proposed Use: 200 units (target)

Land Use Framework

1.13 Storefront Main Street Area



Character:

One block deep along Main Street, and including parking areas behind buildings. Retail uses at ground floor and second floor, and mixed uses of office and residential above. Maintains pedestrian orientation throughout.

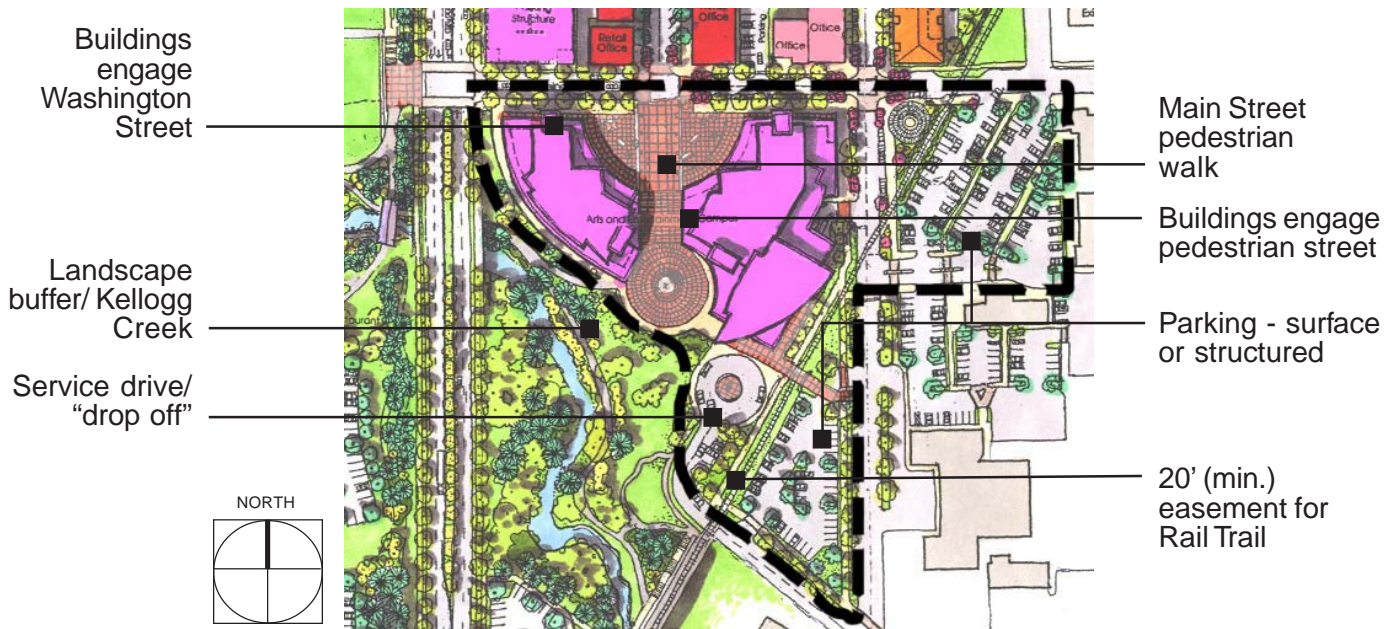
Approximate Area: 8.5 acres

Proposed Use:

retail: 115,000 SF (target)
office: 75,000 SF (target)
residential: 150 units (target)
theater: 25,000 SF

Land Use Framework

1.14 Arts/Entertainment/Office Area



Character:

Envisioned as a campus to anchor the Main Street retail armature. The area will be highly visible from McLoughlin Boulevard, and buildings will address Washington Street. To the south, the campus will be adjacent to the park at Kellogg Creek.

Approximate Area:

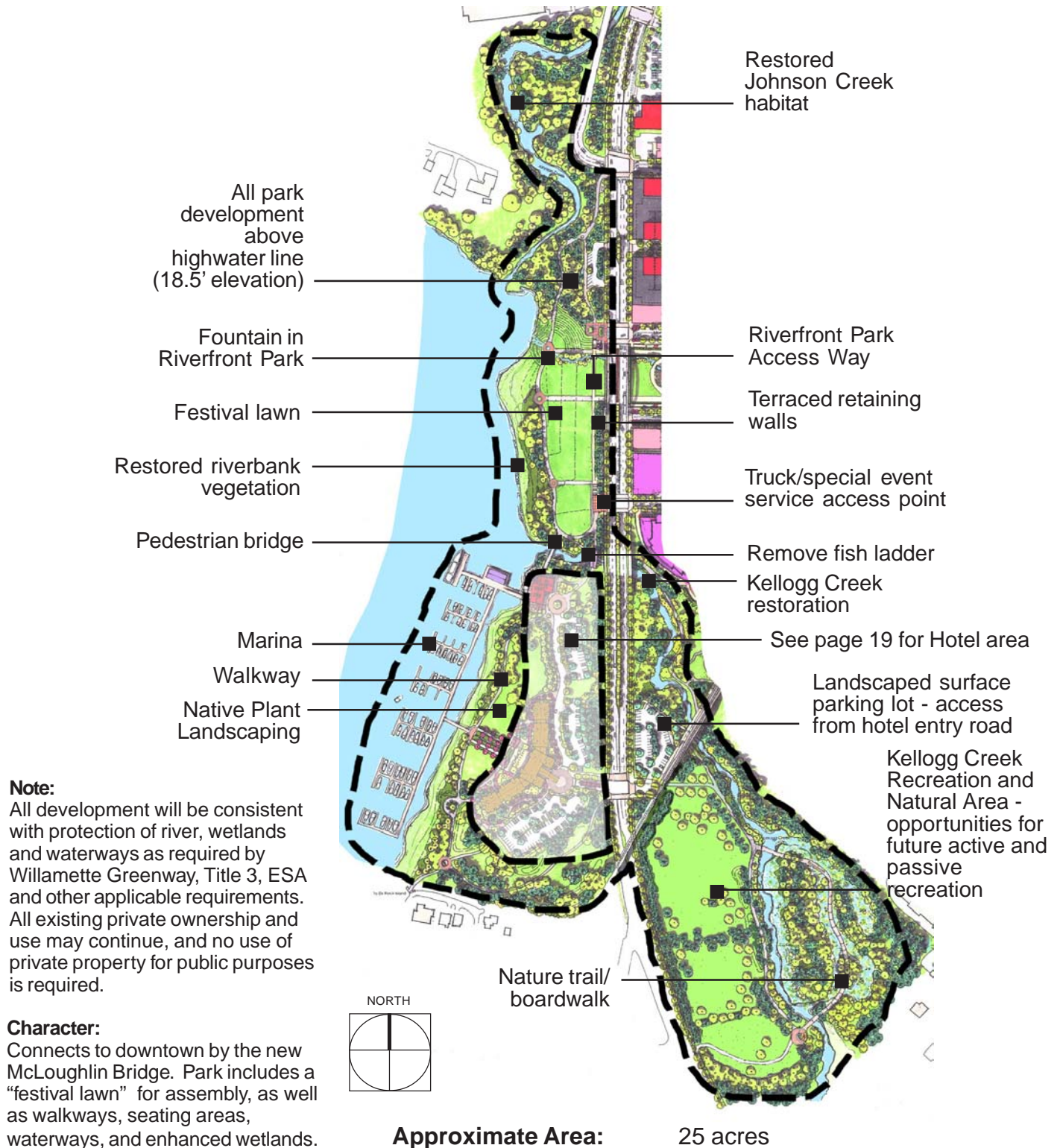
5 acres

Proposed Use:

Retail 50,000 SF (target)
Office 100,000 SF (target)

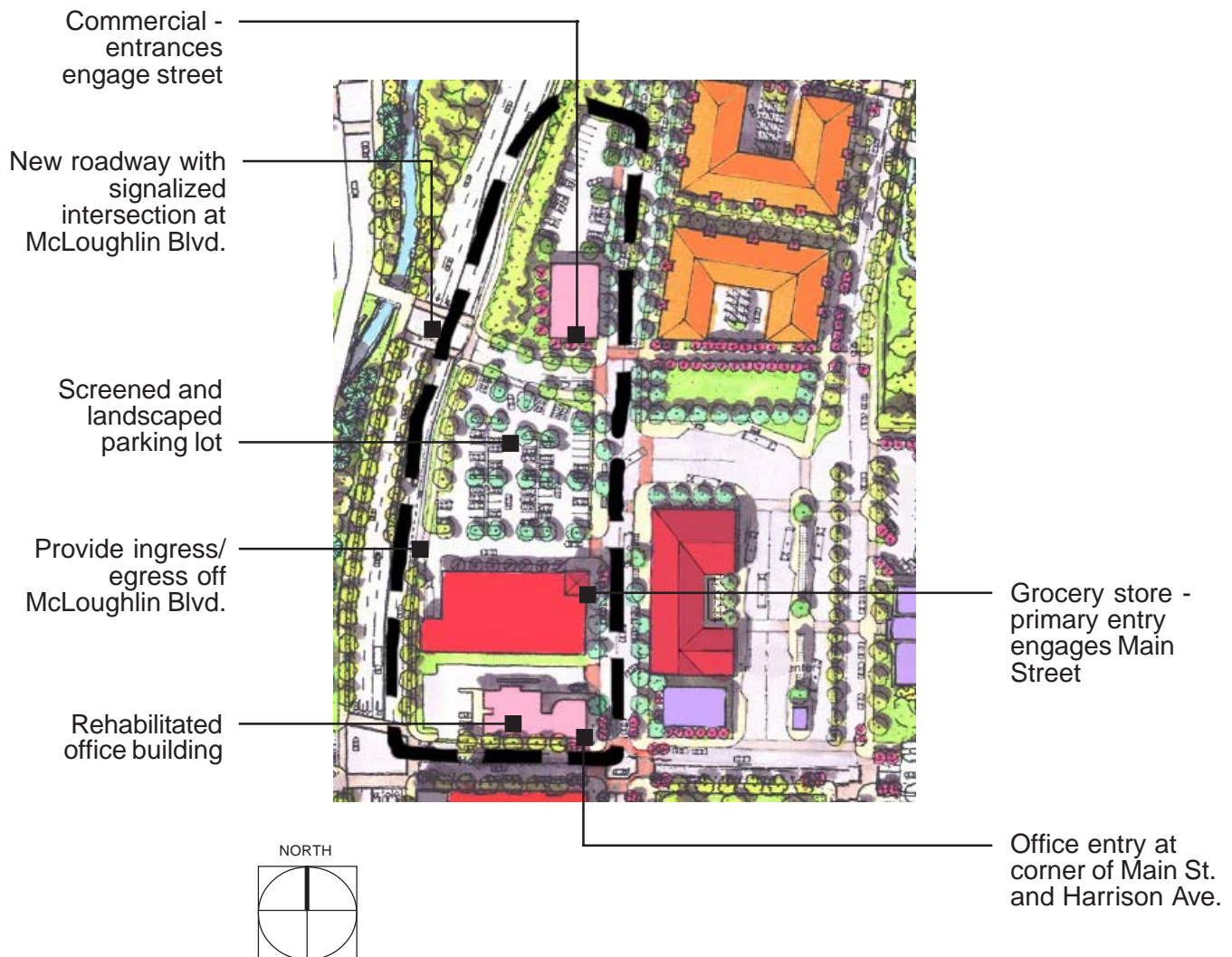
Land Use Framework

1.15 Parks and Open Space Areas



Land Use Framework

1.16 Commercial Area



Character:

An area for commercial development which is auto-accommodating yet maintains a pedestrian-orientation at least at one entrance, and still engages the street right-of-way.

Approximate Area:

2.5 acres

Proposed Use:

Commercial 18,000 SF (target)
Office 20,000 SF (target)

Land Use Framework

1.17 Hotel Area



Character:

Area specifically for a hotel establishment; auto-accommodating but well-landscaped and pedestrian-friendly.

Approximate Area:

6.5 acres

Hotel units:

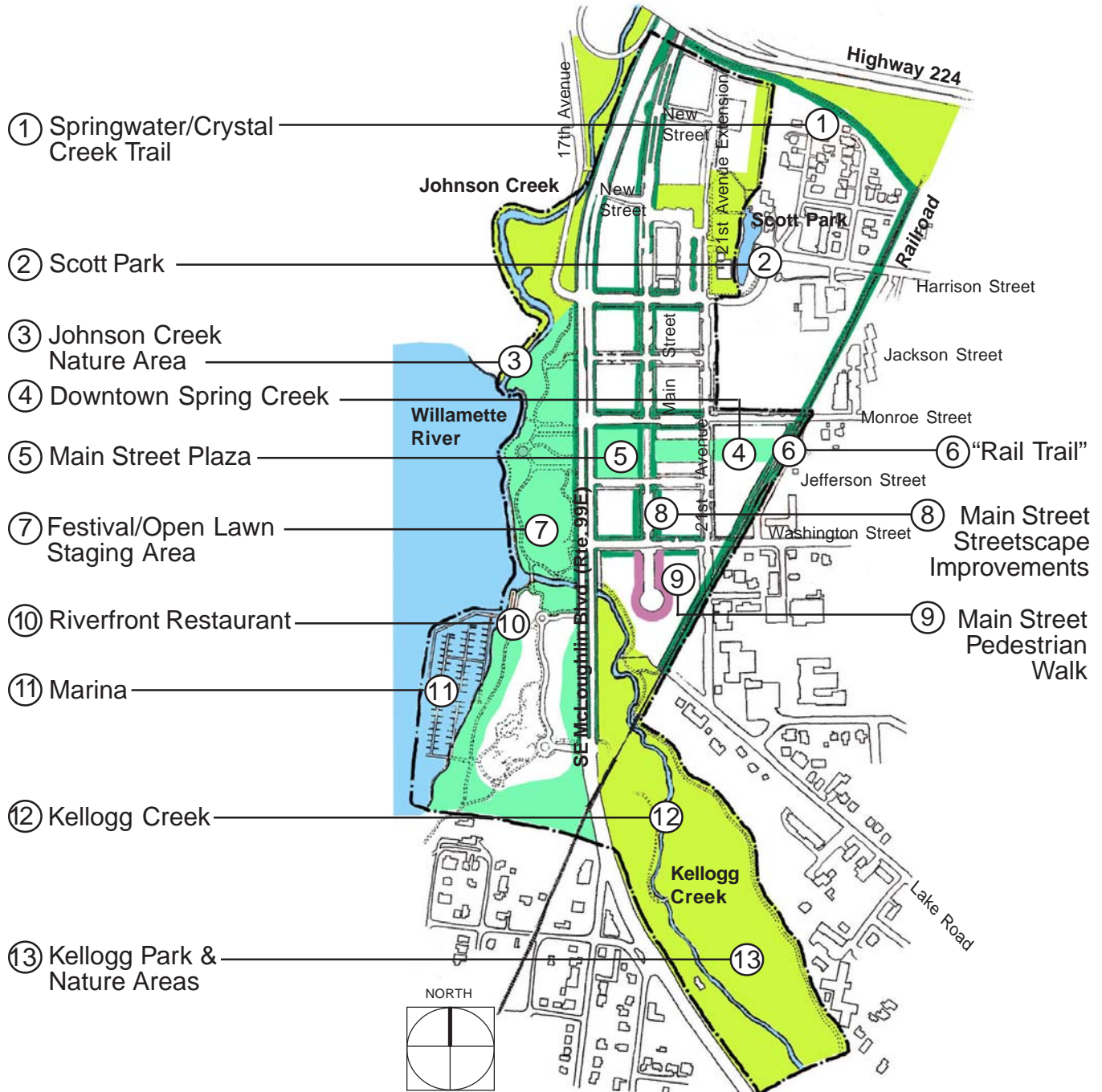
200 rooms (target)

Restaurant:

5,000 SF (max.)

Land Use Framework

1.18 Amenities and Open Space Framework Map



Note: All existing private ownership and use may continue, and no use of private property for public purposes is required.

Land Use Framework

1.19 Amenities and Open Space Framework

The Milwaukie Downtown and Riverfront Land Use Framework Plan includes a system of outdoor open and green spaces weaving around and through the city. There are creeks, restored wetlands, the Willamette River, and a landscaped urban plaza.

Many of the areas are subject to various municipal, state and federal regulations intended to protect environmental and natural resource values; the elements of this framework are intended to be consistent with those regulations.

Spring Creek/Crystal Creek Trail -

Connects the northern end of Milwaukie's downtown with the Portland metro area's Springwater Corridor.

Scott Park - Adjacent to the Ledding Library and the Transit Center Park, as well as to the North Downtown Housing, Scott Park offers lawn and play areas for children and adults. An improved Scott Lake connects through a natural stream to the restored Crystal Creek.

Johnson Creek Nature Area - The creek would be restored to its natural state. The creek connects the Springwater/Crystal Creek Trail to the north with the Riverfront Park and downtown.

Downtown Spring Creek - A small portion of Spring Creek may be diverted into downtown, streaming from the railroad tracks and new "Rail Trail" to the east, through the new Main Street Plaza, to the Riverfront Park.

Main Street Plaza - A paved and landscaped open public gathering space at the center of town, connecting the primary retail street to the Riverfront Park.

"Rail Trail" - Along the railroad tracks runs a trail that, when combined with the Riverfront Park pathways, completes a loop around downtown. The Rail Trail also links the Kellogg Creek area in southern Milwaukie to the Springwater/Crystal Creek Trail to the north, and gives a green edge to the east side of downtown.

Festival/Open Lawn Staging Area - A part of the Riverfront Park is landscaped to accommodate public festivals and assembly.

Main Street/Streetscape Improvement - From the Transit Center south to Washington Street, Main Street benefits from special sidewalk treatments, crosswalks, curb extensions, pedestrian-scaled lighting, street furniture, street trees and planting beds.

Main Street Walk - A pedestrian street with special paving, crosswalks, curb extensions, lighting and street furniture, Main Street Walk terminates in a circle at the arts/office/entertainment campus in the south side of downtown.

Riverfront Restaurant - In Riverfront Park, a new restaurant overlooks the Willamette River and new marina.

Marina - A new public marina and rowing facility is located near the proposed restaurant and hotel buildings which replace the wastewater treatment plant. The marina would accommodate a riverboat or similar vessel.

Kellogg Creek/Kellogg Park and Nature Areas - The creek and wetlands are to be restored to a salmon-friendly state.

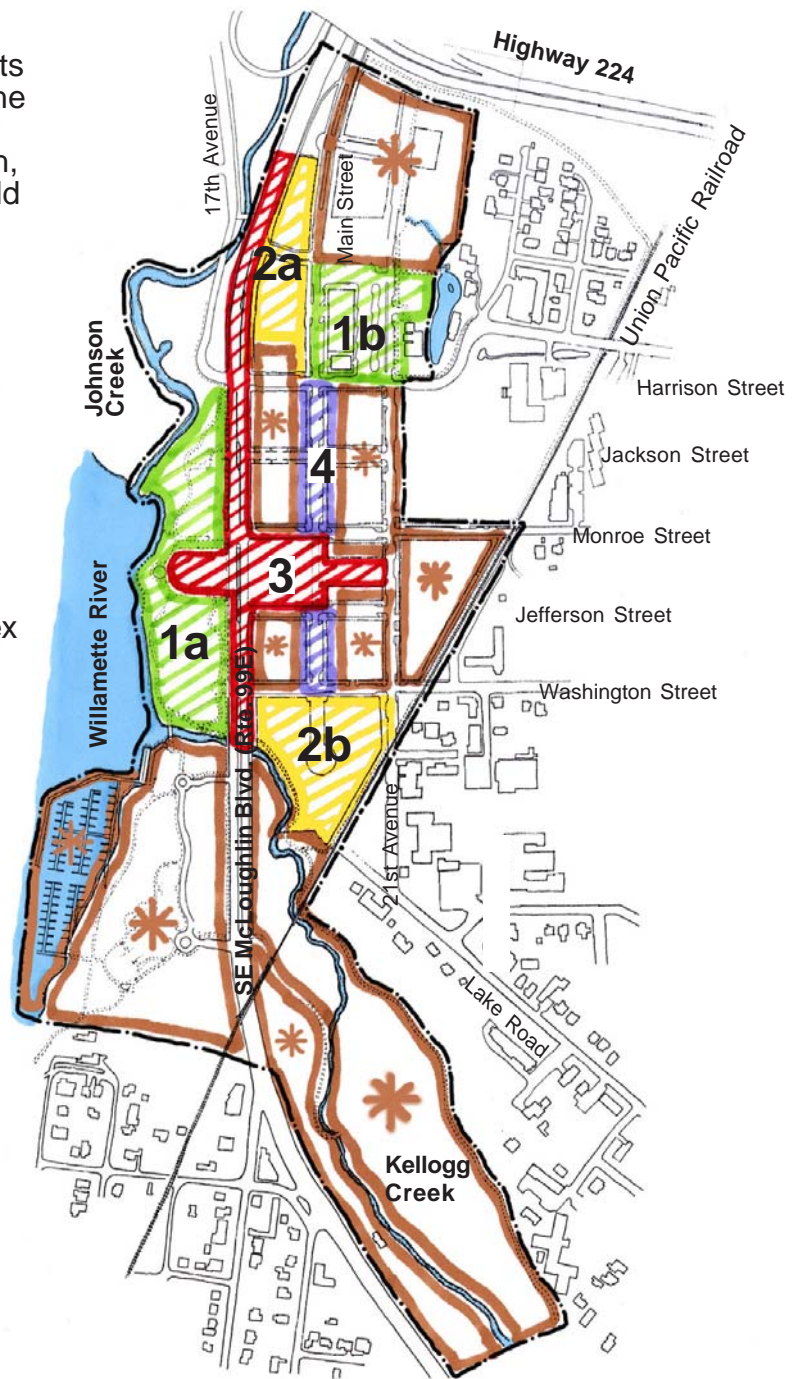
Implementation

2.1 Priority Projects

This diagram sets forth priority projects for the Land Use Framework Plan. The first priority projects will establish a climate of positive change and growth, while larger or more costly ones would occur later.

Priority Projects

- 1a: Riverfront Park Phase 1
- 1b: Bus Transit Center and transit-oriented development
- 2a: Grocery store and parking
- 2b: Arts/entertainment/office complex
- 3: McLoughlin Boulevard improvements, McLoughlin Bridge and parkway connection from Main Street to Riverfront Park
- 4: Main Street streetscape improvements
- *: Residential, commercial and hotel and open space development may occur at any time throughout downtown's revitalization





Proposed Amendments to the Downtown and Riverfront Land Use Framework Plan

**December 2, 2014,
Planning Commission Discussion Draft**
File No. CPA-14-02/ZA-14-02

Please contact Li Alligood, Senior Planner with the City of Milwaukie Planning Department, at 503-786-7627 or alligoodl@milwaukieoregon.gov with questions or comments about the proposed amendments and/or the adoption process.

Background

The proposed amendments to the Downtown and Riverfront Land Use Framework Plan ("Framework Plan") are intended to update the document with adopted policy and completed and planned projects that have emerged since the Plan was adopted in 2000. Ideally, the Framework Plan will become a living document that is regularly updated to reflect completed projects and evolving policies.

Adopted plans that provide the basis for the revisions are:

- Transportation System Plan, adopted in 2007, which adopted a policy to remove the bus transit center from downtown Milwaukie
- Riverfront Park Master Plan, adopted in 2009, which finalized the design for Riverfront Park
- South Downtown Concept Plan, adopted in 2011, which refined the "Arts/Entertainment/Office" area south of Washington St

Projects underway that provide the basis for the revisions are:

- Initiation of master planning for Kronberg Park in 2014 (anticipated completion spring 2015)
- Construction of Riverfront Park Phase II (anticipated completion March 2015)
- Kellogg Bicycle and Pedestrian Bridge (anticipated completion Spring 2015)
- Adams Street Connector (anticipated completion Spring/Summer 2015)

Completed projects that provide the basis for the revisions are:

- Development of the former Bus Transit Center site with North Main Village, a transit-oriented mixed use development, in 2005
- Completion of the Johnson Creek restoration project in 2012
- Initiation of the Kellogg-for-Coho project to restore Kellogg Creek in 2006 (still underway)
- Completion of Riverfront Park Phase I 2012
- Completion of the Portland-to-Milwaukie light rail alignment and station in downtown Milwaukie in 2014/2015

Reader Guide

Commentary

A commentary section precedes each section of code amendments. The commentary provides a non-technical summary of the proposed amendments and identifies the basis of the proposal. The commentary section is labeled as commentary and presented in Comic Sans font (the same font of this sentence).

Amendments

Unless otherwise noted in the document, underlined text is proposed text, and ~~strike through~~ text is existing code language proposed for deletion.

Commentary

The introductory paragraphs of the plan have been revised to reflect that the Framework Plan and implementing regulations changed the existing Town Center Plan Subarea 1 of the Comprehensive Plan; the amendment plan does not suggest further changes.

The plan revisions will result in a more general vision for downtown rather than the specific land uses envisioned for sites in downtown.

Land Use Framework

The Milwaukie Downtown and Riverfront Land Use Framework Plan represents a major opportunity to reinvigorate downtown Milwaukie. With aggressive and focused efforts from both the public and private sectors over the coming years, downtown Milwaukie will be revitalized. This Plan outlines the components necessary to make such a vision of Milwaukie a reality. It foremost builds upon existing assets, capitalizes upon significant existing uses, and features the natural environment. It also sets out a realistic agenda and implementation program that the city and private development can follow.

What This Framework Does

The Land Use Framework is a vision of what can occur in the downtown and riverfront area. It has been tailored to meet Milwaukie's social, economic and development goals and policies while responding to market conditions and the City's overall vision for future growth.

This document serves as an ancillary document to the Milwaukie Comprehensive Plan. On its own, this Framework is not a regulatory document. Any part of the Framework that is intended to have a binding effect will have to be adopted as part of, or pursuant to, a code to have regulatory effect. ~~The Land Use Framework suggests changes to the existing Town Center Plan Subarea 1 of the Comprehensive Plan.~~

This Framework establishes and guides the development of publicly and privately owned parcels of land, ~~and outlines specific land uses.~~ Nothing in this framework authorizes public use of private property. The Zoning Ordinance provisions regarding permissible uses prevail over any inconsistent provision in this Downtown and Riverfront Land Use Framework.

This Land Use Framework allows existing businesses to remain as long as their owners wish. All privately owned property designated as a public use in this Framework - for example, parks or trails - will continue to be in a "private ownership" zoning category, and will not be rezoned for public use until it is acquired by a public entity.

If an existing business does not conform to the new planning area's allowable uses and development regulations, it will be allowed to continue to operate, as well as to expand and rebuild, in accordance with the Downtown Design Standard Threshold Regulations of the City of Milwaukie's Zoning Ordinance.

Commentary

Figure 1.3, which shows a detailed schematic of proposed land uses and development in downtown, is being deleted to reflect the more consistent nature of the proposed downtown regulations.

Subsequent graphics and sections have been renumbered to reflect this deletion.

1.1 Aerial Photograph of Existing Downtown

[GRAPHIC PLACEHOLDER]

1.2 Downtown Planning Area Map

Subarea 1 of the 1997 Town Center Master Plan

[GRAPHIC PLACEHOLDER]

1.3 Illustrative Plan

[GRAPHIC PLACEHOLDER]

1.34 Framework Elements

The Milwaukie Downtown and Riverfront Land Use Framework recognizes and builds upon the rich character and history of our town. It celebrates the diversity and spirit of the people and places of Milwaukie, and reinforces those special qualities.

City officials, community leaders and area businesses have known for a long time that Milwaukie's historic downtown, nestled at the edge of the Willamette River, can and should be a vital center for the community's economic and social activity. Starting in 1999, the City of Milwaukie began a process to develop a concept for downtown Milwaukie that reflects the vision of the people who live and work there. During a number of community design forums, citizens have tested various aspects of the plan, with local residents ranking their priorities for the downtown area.

This Land Use Framework represents the leadership of a skilled volunteer Riverfront Board and the input of the more than 2000 community members who have attended meetings, returned surveys, provided focus and ideas, and directed the plan.

~~The Recipe that Works~~

~~This Land Use Framework represents a process derived from a recipe of fundamental concepts that has worked successfully in other cities, equal in size to Milwaukie and with similar challenges. In those cities, the public and private sectors worked together to make revitalization happen. They developed a plan, and subsequently residents and government officials did what it took to make their plans work. Their efforts paid off. This Land Use Framework will do the same for Milwaukie.~~

[PHOTO PLACEHOLDER: Downtown view from Willamette River]

[PHOTO PLACEHOLDER: Historic Masonic Lodge]

[PHOTO PLACEHOLDER: Lot Whitcomb, 1850]

Commentary

The **Fundamental Concepts** have been updated to reflect the South Downtown Concept Plan and the new public plaza it proposes.

Implementation has been updated to reflect the incorporation of the South Downtown Concept in the revised document.

Fundamental Concepts

Milwaukie's unique character is at the heart of the Downtown and Riverfront Framework. The Framework reconnects Milwaukie to the Willamette River, knitting together the seam of McLoughlin Boulevard. It creates the new Riverfront Park as the city's "living room." And it calls for revitalizing historic buildings while designing new structures to harmonize with the town's historic character.

Anchors and attractors are used to build upon existing resources, and to strengthen the Main Street "retail spine armature." South Downtown is refreshed with a new public plaza and light rail station. Thus the Framework is a blueprint to make Milwaukie and its downtown a vital, livable and sustainable community.

Amenities and Open Spaces

Milwaukie is fortunate to have a setting that inspires its citizens, that offers history, beauty and vitality. The Willamette River, Johnson Creek, and Kellogg Creek all provide beautiful natural borders to the downtown area. The Milwaukie Downtown and Riverfront Framework capitalizes on these natural resources, by restoring the creeks and improving the riverfront for public use.. Thus both the town and the natural areas are reinvigorated.

Implementation

This Framework coordinates with Milwaukie's existing planning efforts and regulations. The Milwaukie Comprehensive Plan provides the guiding policies to manage the city's physical elements. Ancillary planning documents are part of the Comprehensive Plan. These include the Milwaukie Vision Statement of 1995 and the Town Center Master Plan of 1997. The latter responds to Milwaukie's designation as one of Metro's 2040 Growth Concept Town Centers. The Downtown and Riverfront Framework is a conceptual document. Therefore, various sections have been adopted as part of, or referenced by the existing Milwaukie Zoning Ordinance, Subdivision Ordinance, or Municipal Code to be effective as a regulatory document.

The Downtown and Riverfront Framework responds to environmental policies including Metro's Title 3 and 13 Requirements and the Federal Endangered Species Act.

This Framework also reflects the adopted South Downtown Concept Plan, which serves as a refinement plan to the area south of Washington Street that will be most directly influenced by the new light rail station.

Phasing and Financing Strategy

The Framework is based on the idea ~~makes this bold statement:~~ that investing in the future of Milwaukie makes sense. Investment offers employment and opportunity for all the citizens of Milwaukie; it enriches the town literally and figuratively. Smart public improvements stimulate substantial private investment. Thus, investing today in a better quality of life for downtown Milwaukie makes sense for the whole town, for years to come.

Commentary

Renumbered 1.4 Our Guiding Principles – A Touchstone

This section has been updated to reflect the confirmation of the guiding vision for downtown through the 2013 *Fresh Look Milwaukie: Downtown Road Map* project.

1.45-Our Guiding Principles - A Touchstone

The community's vision for downtown was reaffirmed through public outreach in 2013. In 1999, the City of Milwaukie began a process to develop its vision of the downtown and riverfront. Throughout this community effort, the Framework was developed in accordance with and responded to the following guiding principles:

Creating a livable community:

- Provide for residents, workers and visitors alike.
- Provide for people of all ages, cultures, ethnic groups and incomes.
- Provide cultural arts and entertainment facilities.
- Provide significant open spaces and connections to the riverfront.
- Provide for specific "programmatic" requirements, such as parking or visibility from major roadways.

Ensuring economic success:

- Efficiently maximize current investment in infrastructure.
- Spur further private investment.
- Recognize and respond to the current marketplace.
- Establish a strategy for capturing unrealized market niches.
- Complement, protect and promote the continued growth and vitality of current businesses.

[PHOTO PLACEHOLDER: no caption, picture of canoe and trees at river]

Commentary

Renumbered 1.5 Fundamental Concepts Map

This map has been updated to reflect the expansion of the plan's focus on Main Street to McLoughlin Blvd and 21st Avenue. Revisions include:

- Identifying McLoughlin Blvd as a commercial corridor
- Identifying 21st Ave as a commercial, office, and retail corridor
- Establishing gateways at either end of downtown on McLoughlin Blvd
- Clarifying that connections between downtown and the riverfront should occur at key pedestrian streets with signalized crossings (Harrison St, Monroe St, and Washington St)
- Identifying the area north of Harrison St as a potential "urban village" development area
- Identifying the South Downtown Concept planning area

New 1.6 South Downtown Concept Plan Area

The South Downtown Concept Plan was adopted by resolution in 2011. Including it in the Framework Plan formally implements the key concepts of this plan.

1.56-Fundamental Concepts Map

[GRAPHIC PLACEHOLDER]

1.6 South Downtown Concept Plan Area

[GRAPHIC PLACEHOLDER]

Commentary

1.7 Fundamental Concepts

Anchors and Attractors

Revisions reflect that fact that a grocery store, while a desired anchor/attractor use, is not the only use that would result in increased foot traffic.

A reference to the South Downtown Plaza reflects the adopted South Downtown Concept Plan and the role of the light rail station and plaza as anchors and attractors that will be used by many people on a daily basis.

New anchor uses are revised to reflect the potential of a "village concept" area in north downtown and the new light rail station and public plaza in south downtown to generate significant pedestrian traffic downtown.

The Main Street Retail Armature

This concept has been renamed the "Main Street Retail Spine" to more clearly reflect its role as the primary retail street in downtown.

The language is updated to reflect the expansion of the Main Street Retail Spine from four blocks along Main Street to the 5-6 blocks between Harrison St and the light rail station.

References to the Main Street Plaza have been revised to reflect the refined concept adopted by the South Downtown Concept Plan, which includes a plaza at the south end of Main Street rather than the center of Main Street.

1.7 Fundamental Concepts

Anchors and Attractors

The keystone to building a successful downtown is to build upon existing resources - the quality stores and offices that we already have - and supplement these with anchors and attractors - places used by hundreds of people on a daily basis. A grocery store, for example, ~~will~~ could generate considerable foot traffic, which ~~will~~ would in turn provide additional customers for downtown businesses. A public plaza at the south end of Main Street, as depicted in the South Downtown Concept Plan, will provide gathering and event space, revitalize the area, and complement surrounding commercial uses.

The framework includes key elements which will be necessary to achieve these goals. New "anchor" and "attractor" uses are as follows:

- Village housing opportunity area north of Harrison St
- New light rail station and public plaza in South Downtown
- ~~Bus transit center.~~
- ~~Grocery store across Main Street from the transit center.~~
- ~~Arts, entertainment and office "campus" of buildings at the southern end of Main Street, including a graphics-oriented higher education facility.~~

The Main Street "Retail Spine Armature"

Reactivating Main Street is a major focus - re-establishing and strengthening a lively storefront retail character with a pedestrian emphasis and 24-hour use.

The framework establishes an environment in which people can shop, work, live and socialize along Main Street. It addresses and repairs the fundamental problems that have drained downtown of its vitality. The Framework suggests adding to or filling in blocks with new uses and in some instances tearing down buildings and starting over again.

~~In the four blocks between these two anchors~~ Along Main Street, the fabric of ground floor retail establishments will create a lively flow of pedestrian activity. The ~~Main Street~~ retail framework armature ensures that promotes a healthy retail street that includes:

- Retail on both sides of the street.
- Continuous retail facades with no interruptions.
- On-street parking in front of retail.
- Anchor retail at both ends of Main Street.
- Retail on all four corners of intersections.
- A pedestrian loop.
- Safe, signalized pedestrian crossings as needed.
- Pedestrian-friendly amenities - wide sidewalks, landscaping, benches.

This north-south flow of activity will be further enlivened where it intersects with the new Main Street Plaza at Main Street and Adams Street, which provides views of the Willamette River and a location for events and activities such as the Farmers Market ~~in the block between Monroe and Jefferson Streets and leading directly to the Willamette River and the new Riverfront Park.~~

Commentary

McLoughlin Blvd Commercial Corridor

This is a new section reflecting public feedback about the importance of McLoughlin Blvd as a commercial corridor and connection between downtown Milwaukie and Riverfront Park.

21st Ave Commercial, Office, and Retail Corridor

This is a new section reflecting public feedback regarding the importance of 21st Ave as a pedestrian corridor.

Connecting to the River

This section has been revised to reflect the more general vision rather than individual development concepts.

South Downtown Planning Area

This section has been revised to incorporate the key components of the South Downtown Concept Plan, which refined the concept for the "Arts and Entertainment Anchor" subarea of the 2000 Framework Plan.

McLoughlin Blvd Commercial Corridor

Enhancing McLoughlin Blvd as the primary entry point to downtown Milwaukie from the north, south, and riverfront trails provides a sense of place for visitors to downtown and provides a visual connection between downtown and the riverfront.

The framework establishes an environment that is visually attractive and accommodates the pedestrian. Along McLoughlin Blvd, new buildings will provide a comfortable pedestrian environment while welcoming visitors to the riverfront into downtown. The framework promotes a corridor that includes:

- Parking beside or behind buildings
- Continuous "street wall"
- Strong intersections at key signalized crossings
- Gateway treatments at Scott Street and Sparrow Street to make it clear that visitors are entering downtown Milwaukie

21st Ave Mixed Use Corridor

With the construction of the light rail station, 21st Ave is a key pedestrian connection between the station and the bus shelter area on Jackson St. It is also a direct connection to key civic institutions including Milwaukie High School, the Waldorf School, City Hall, and Ledding Library.

The framework establishes an environment that is pedestrian-friendly, vibrant, and attractive, while focusing retail uses on the Main St spine. The 21st Ave corridor includes:

- Live/work units, residential, and mixed use development
- Pedestrian-friendly amenities - wide sidewalks, landscaping, benches.

Connecting to the River

The new Riverfront Park will be the location for special events such as "Festival Daze," holiday celebrations and community assemblies. Safe pedestrian and bicycle crossings between downtown and Riverfront Park and multiuse trails are important. Pedestrian connections will be strengthened at key east/west streets to allow pedestrian and bicycle access to the park. At its southern end will be a new hotel, adjacent to the new public marina and rowing facility.

South Downtown Planning Area

The South Downtown Concept Plan refined the vision for the area south of Washington St. The key concepts of the Plan include:

- The development in downtown is fine-grained and human-scale.
- An active, vibrant public plaza provides the central anchor for South Downtown, and provides views to the Willamette River.
- The plaza includes an interactive fountain, seating, and is graded toward the river.
- South Downtown is located in close proximity to several natural areas, including Kellogg Lake, Dogwood Park, Kronberg Park, and Riverfront Park. Access to these natural areas should be provided via walking trails and/or McLoughlin Blvd crossings.

[PHOTO PLACEHOLDER: Café seating on a plaza]

[PHOTO PLACEHOLDER: Street used as farmers market]

Commentary

1.8 Land Use Framework

This graphic has been deleted to reflect the proposed combination of 4 area-specific zones into 1 general Downtown Mixed Use zone.

Renumbered 1.6 Key Land Use Features

Revitalizing Main Street and downtown:

Additions:

- The role of pedestrian-friendly improvements throughout downtown is formalized as a key revitalizing agent for downtown.
- A reference to the South Downtown Plaza reflects the adopted South Downtown Concept Plan and the role of the light rail station and plaza as anchors and attractors that will be used by many people on a daily basis.

Revisions:

- Minor revision to make references to "landmarks" consistent throughout planning documents

Deletions:

- A grocery store, while a desired anchor/attractor use, is not the only use that would result in increased foot traffic.
- The proposed TriMet Bus Transit Center site has been developed with the North Main Village development.
- The Arts and Entertainment Campus concept has been refined through the South Downtown Concept Plan.

Reconnecting to the River:

Deletions:

- New Public Marina and Restaurant and New Riverfront hotel have been deleted to reflect the more general policy role of the plan, rather than specific development types and locations.

Restoring Natural Areas and Parks:

Additions:

- Maintaining the Johnson Creek restoration completed in 2012 has been added as a key land use feature

Revisions:

- Efforts to restore Kellogg Creek are underway, and should be continued.

Deletions:

- There are no plans in place to divert a portion of Spring Creek into a downtown park
- Johnson Creek restoration was completed in 2012

~~1.8 Land Use Framework~~

[GRAPHIC PLACEHOLDER]

~~1.68~~ Key Land Use Features

Revitalizing Main Street and downtown:

- Main Street improvements - shops, services, and family wage jobs
- Save landmarks ~~Landmarks~~ - to preserve history and heritage
- Pedestrian-friendly improvements throughout downtown
- New South Downtown plaza and light rail station
- ~~New Grocery Store to anchor Main Street~~
- ~~New Tri-Met Bus Transit Center~~
- ~~New Arts and Entertainment Campus~~

Reconnecting to the River:

- New McLoughlin Bridge - to knit the seam between the downtown and the river
- New Riverfront Park - the City's living room
- New and enhanced pedestrian crossings
- ~~New Public Marina and restaurant~~
- ~~New Riverfront hotel to replace the water treatment plant~~

Restoring Natural Areas and Parks:

- ~~Downtown stream - divert a portion of Spring Creek into downtown park~~
- Restore Spring Creek
- ~~Restore Johnson Creek~~
- Maintain Johnson Creek restoration
- Continue efforts to restore ~~Restore~~ Kellogg Creek

Commentary

Provide Quality Housing:

Additions:

- New text reflects the proposed deletion of the Downtown Residential zone and allowance for various residential types throughout downtown.

Deletions:

- The proposed deletion of the Downtown Residential zone would not require residential development to be focused in these areas.

Proposed Amendment

Providing Quality Housing:

- Provide a variety of quality housing types including rowhouses, townhomes, apartments and condominiums (both stand-alone and as part of mixed use development) and live/work spaces.
- ~~To the North, townhomes and apartments engaging new parks, near Spring Creek and trail to Spring Water Corridor~~
- ~~To the South, townhomes and apartments along landscaped creek and Rail Trail~~

Commentary

1.10 Land Use Descriptions

This entire section (1.10 - 1.18) has been deleted to reflect the more general policy role of the Framework Plan, rather than specific development types and locations.

1.10 Land Use Descriptions

Downtown Milwaukie includes six established and emerging planning areas, each with distinctive physical characteristics and varying uses. While they share a singular overall area—the downtown of Milwaukie—they serve various social, cultural, and economic roles. The goal of the Downtown and Riverfront Plan is to secure a future which binds all of these existing and potential areas into a coherent downtown while enabling each individual area to maintain or develop a distinctive identity.

[PHOTO PLACEHOLDER: Bus Transit Center]

[PHOTO PLACEHOLDER: Storefront Main Street]

[PHOTO PLACEHOLDER: Apartments—North Housing]

1.11 Housing North Area

[GRAPHIC PLACEHOLDER]

UNDER GRAPHIC TEXT:

Character:

Multiple-family residences: ownership/condominiums and rental (including townhouses and apartments).

Approximate Area: 10 acres

Proposed Use: 25 townhouse units (target)

225 units apts/condominiums (target)

Total units: 250 (target)

1.12 Housing South Area

[GRAPHIC PLACEHOLDER]

UNDER GRAPHIC TEXT:

Character:

Multiple-family residence types: rental apartments or condominiums.

Approximate Area: 2 acres

Proposed Use: 200 units (target)

1.13 Storefront Main Street Area

[GRAPHIC PLACEHOLDER]

UNDER GRAPHIC TEXT:

Character:

One block deep along Main Street, and including parking areas behind buildings. Retail uses at ground floor and second floor, and mixed uses of office and residential above. Maintains pedestrian orientation throughout.

Approximate Area: 8.5 acres

Proposed Use: retail: 115,000 SF (target)

Commentary

Proposed Amendment

_____ office: 75,000 SF (target)
 _____ residential: 150 units (target)
 _____ theater: 25,000 SF

1.14 Arts/Entertainment/Office Area

[GRAPHIC PLACEHOLDER]

UNDER GRAPHIC TEXT:

Character:

Envisioned as a campus to anchor the Main Street retail armature. The area will be highly visible from McLoughlin Boulevard, and buildings will address Washington Street. To the south, the campus will be adjacent to the park at Kellogg Creek.

Approximate Area: 5 acres

Proposed Use: _____ Retail 50,000 SF (target)
 _____ Office 100,000 SF (target)

1.15 Parks and Open Space Areas

[GRAPHIC PLACEHOLDER]

UNDER GRAPHIC TEXT:

Note:

All development will be consistent with protection of river, wetlands and waterways as required by Willamette Greenway, Title 3, ESA and other applicable requirements. All existing private ownership and use may continue, and no use of private property for public purposes is required.

Character:

Connects to downtown by the new McLoughlin Bridge. Park includes a "festival lawn" for assembly, as well as walkways, seating areas, waterways, and enhanced wetlands.

Approximate Area: 25 acres**1.16 Commercial Area**

[GRAPHIC PLACEHOLDER]

UNDER GRAPHIC TEXT:

Character:

An area for commercial development which is auto-accommodating yet maintains a pedestrian-orientation at least at one entrance, and still engages the street right-of-way.

Approximate Area: 2.5 acres

Proposed Use: _____ Commercial 18,000 SF (target)
 _____ Office 20,000 SF (target)

Commentary

~~1.17 Hotel Area~~

~~[GRAPHIC PLACEHOLDER]~~

~~UNDER GRAPHIC TEXT:~~

~~Character:~~

~~Area specifically for a hotel establishment; auto-accommodating but well landscaped and pedestrian friendly.~~

~~Approximate Area:~~ ~~6.5 acres~~

~~Hotel units:~~ ~~200 rooms (target)~~

~~Restaurant:~~ ~~5,000 SF (max.)~~

Commentary

Renumbered 1.9 Amenities and Open Space Framework Map

This section has been updated to reflect updated policies and completed projects.

Additions:

- A plan for Riverfront Park was adopted in 2009. This language clarifies its role as the primary open space in downtown Milwaukie.
- The City received grant funding to construct a shared bicycle/pedestrian facility across Kellogg Lake, linking Dogwood Park to the east with Kronberg Park to the west.
- The Trolley Trail is currently under construction and connects downtown Milwaukie with points south.

Revisions:

- The Main Street Plaza has been replaced by the South Downtown Plaza in the Land Use Framework.

1.948-Amenities and Open Space Framework Map

[GRAPHIC PLACEHOLDER]

1.1049-Amenities and Open Space Framework

The Milwaukie Downtown and Riverfront Land Use Framework Plan includes a system of outdoor open and green spaces weaving around and through the city. There are creeks, restored wetlands, the Willamette River, and a landscaped urban plaza.

Many of the areas are subject to various municipal, state and federal regulations intended to protect environmental and natural resource values; the elements of this framework are intended to be consistent with those regulations.

1. Scott Park - Adjacent to the Ledding Library and ~~the Transit Center Park, as well as to the North Downtown Housing Main Village~~, Scott Park offers lawn and play areas for children and adults. An improved Scott Lake connects through a natural stream to the restored Crystal Creek.

2. Johnson Creek Nature Area –The creek connects the Springwater/Crystal Creek Trail to the north with the Riverfront Park and downtown. ~~The creek would be restored to its natural state.~~

3. City Hall Plaza & Sculpture Garden – The City Hall plaza and Sculpture Garden provide places to stop, rest, and people-watch.

4. Main Street/Streetscape Improvement – ~~From the Transit Center south to Washington Street, Downtown streets~~ Main Street benefits from wider sidewalks special sidewalk treatments, crosswalks, curb extensions, pedestrian-scaled lighting, street furniture, street trees and planting beds. Main Street is emphasized as the primary retail street with special sidewalk treatments and wider sidewalks allowing for sidewalk café seating and retail displays.

5. Riverfront Park – The park is being improved and will become the City’s “living room” including a festival lawn, overlooks for viewing the river, pathways for pedestrians and cyclists, improvements to the boat launch and parking, and extensive riverside plantings. Located adjacent to downtown and nestled between the Willamette River, Kellogg Creek and Johnson Creek, Riverfront Park is the focus of Milwaukie’s open space framework.

6. South Downtown Plaza & Adams Street Connector – Create a major plaza forming the core and focus of the South Downtown. The plaza will have views to the Willamette River, retail spaces to the east, connections natural areas, and Main Street running through the center. During events, the plaza and a portion of Main Street will be closed to vehicular traffic. The Adams Street Connector is a pedestrian green street that provides a pedestrian connection between the light rail station and the South Downtown Plaza.

7. Dogwood Park –Dogwood Park is expanded and integrated to the north and east with the South Downtown Plaza and Main Street streetscape improvements, and to the south and west with the Kellogg Nature Area.

8. Kellogg Bicycle & Pedestrian Bridge - Connecting the open spaces of Dogwood Park and Kronberg Park and the Trolley Trail beyond with a bicycle/pedestrian bridge.

9. Kronberg Park ~~Kellogg Creek/Kellogg Park~~ and Kellogg Nature Areas – Planning for improvement of Kronberg Park, and restore the creek and wetlands to a salmon-friendly state. Restoration of Kellogg Creek will provide nature trails through native riparian banks and along the restored stream. The creek and wetlands are to be restored to a salmon-friendly state.

Commentary

Additions:

- The Trolley Trail is currently under construction and connects downtown Milwaukie with points south.

Deletions:

- There are no plans to divert Spring Creek through downtown to Riverfront Park.
- The "Rail Trail" was planned for the current light rail alignment, and is no longer feasible.
- The Festival/Open Lawn Staging Area has been incorporated into the design for Riverfront Park (see Additions below).
- The Main Street Walk was located in the "Arts/Office/Entertainment" campus, which was subsequently refined by the 2011 South Downtown Concept Plan. Main Street is now planned to remain open to both pedestrian and vehicular traffic.

10. Trolley Trail – The 6-mile Trolley Trail multiuse path stretches between Riverfront Park in downtown Milwaukie to Gladstone to the south.

~~**Downtown Spring Creek** – A small portion of Spring Creek may be diverted into downtown, streaming from the railroad tracks and new "Rail Trail" to the east, through the new Main Street Plaza, to the Riverfront Park.~~

~~**Main Street Plaza** – A paved and landscaped open public gathering space at the center of town, connecting the primary retail street to the Riverfront Park.~~

~~**"Rail Trail"** – Along the railroad tracks runs a trail that, when combined with the Riverfront Park pathways, completes a loop around downtown. The Rail Trail also links the Kellogg Creek area in southern Milwaukie to the Springwater/Crystal Creek Trail to the north, and gives a green edge to the east side of downtown.~~

~~**Festival/Open Lawn Staging Area** – A part of the Riverfront Park is landscaped to accommodate public festivals and assembly.~~

~~**Main Street Walk** – A pedestrian street with special paving, crosswalks, curb extensions, lighting and street furniture, Main Street Walk terminates in a circle at the arts/office/entertainment campus in the south side of downtown.~~

~~**Riverfront Restaurant** – In Riverfront Park, a new restaurant overlooks the Willamette River and new marina.~~

~~**Marina** – A new public marina and rowing facility is located near the proposed restaurant and hotel buildings which replace the wastewater treatment plant. The marina would accommodate a riverboat or similar vessel.~~

Commentary

New heading 2.0 Implementation

2.1 Projects

The graphic has been updated to reflect current and planned capital projects in and connected to downtown Milwaukie, as well as to remove projects that have been completed.

Because the projects have not been prioritized through a public process, the title has been shortened to "Projects."

Implementation

2.1 ~~Priority~~ Projects

[GRAPHIC PLACEHOLDER]

This diagram sets forth ~~priority~~ key projects for the Land Use Framework Plan. The first ~~priority~~ key projects will establish a climate of positive change and growth, while larger or more costly ones would occur later.

~~Priority~~ Projects

1a: Riverfront Park Phases III-IV 4

2. 17th Avenue bicycle and pedestrian connection

1b: ~~Bus Transit Center and transit-oriented development~~

2a: ~~Grocery store and parking~~

2b: ~~Arts/entertainment/office complex~~

3: McLoughlin Boulevard improvements north of Harrison St, McLoughlin Bridge

4: Streetscape improvements

5: South Downtown / Main Street Plaza

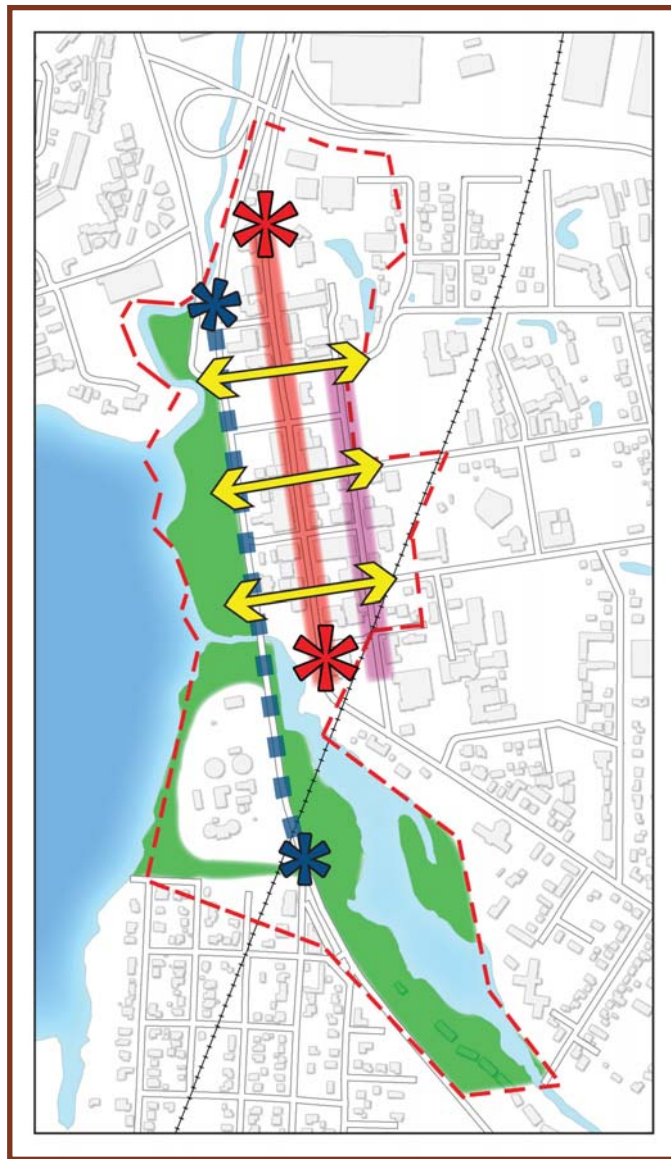
6: Adams Street Connector

7. Kellogg-for-Coho (restoration of Kellogg Creek)

*: ~~Residential, commercial and hotel and open space development may occur at any time throughout downtown's revitalization~~

Milwaukie Downtown and Riverfront Land Use Framework Plan

Ancillary Document to the
Milwaukie Comprehensive Plan



Adopted Ord. #1880 September 19, 2000
Last Rev. Ord. #____, adopted MONTH DAY, 2015

Title page and/or
Mayor's letter placeholder

Acknowledgements placeholder

TOC
placeholder

1. Land Use Framework

The Milwaukie Downtown and Riverfront Land Use Framework Plan represents a major opportunity to reinvigorate downtown Milwaukie. With aggressive and focused efforts from both the public and private sectors over the coming years, downtown Milwaukie will be revitalized. This Plan outlines the components necessary to make such a vision of Milwaukie a reality. It foremost builds upon existing assets, capitalizes upon significant existing uses, and features the natural environment. It also sets out a realistic agenda and implementation program that the city and private development can follow.

What This Framework Does

The Land Use Framework is a vision of what can occur in the downtown and riverfront area. It has been tailored to meet Milwaukie's social, economic and development goals and policies while responding to market conditions and the City's overall vision for future growth.

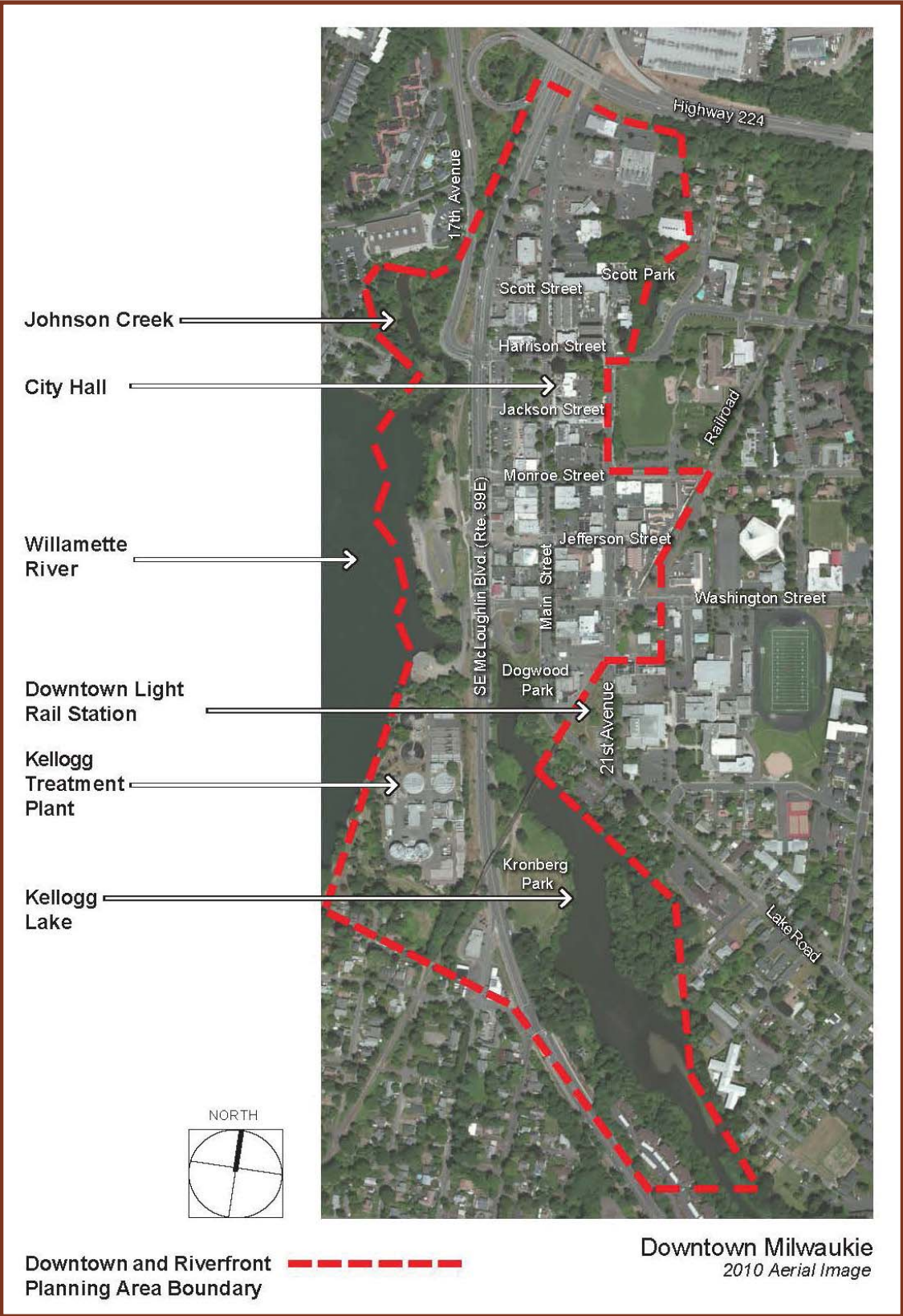
This document serves as an ancillary document to the Milwaukie Comprehensive Plan. On its own, this Framework is not a regulatory document. Any part of the Framework that is intended to have a binding effect will have to be adopted as part of, or pursuant to, a code to have regulatory effect.

This Framework establishes and guides the development of publicly and privately owned parcels of land. Nothing in this framework authorizes public use of private property. The Zoning Ordinance provisions regarding permissible uses prevail over any inconsistent provision in this Downtown and Riverfront Land Use Framework.

This Land Use Framework allows existing businesses to remain as long as their owners wish. All privately owned property designated as a public use in this Framework - for example, parks or trails - will continue to be in a "private ownership" zoning category, and will not be rezoned for public use until it is acquired by a public entity.

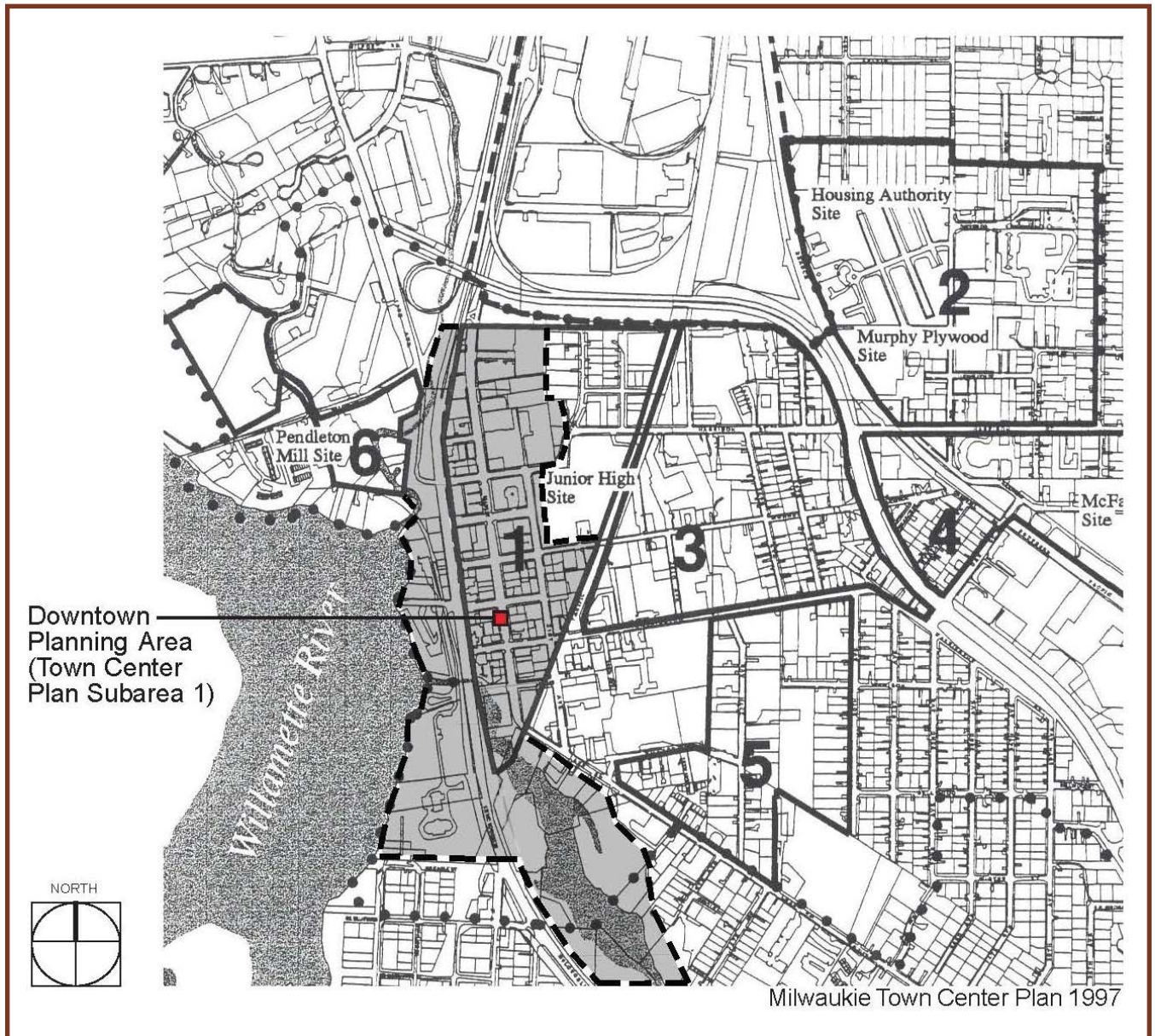
If an existing business does not conform to the new planning area's allowable uses and development regulations, it will be allowed to continue to operate, as well as to expand and rebuild, in accordance with the Downtown Design Standard Threshold Regulations of the City of Milwaukie's Zoning Ordinance.

1.1 Aerial Photograph of Existing Downtown



1.2 Downtown Planning Area Map

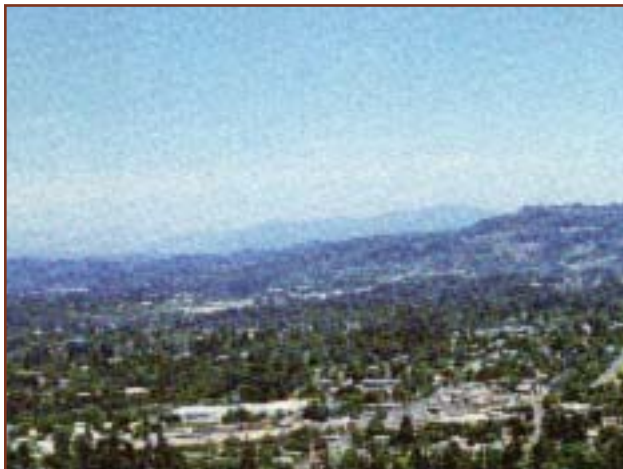
Subarea 1 of the 1997 Town Center Master Plan



1.3 Framework Elements

The Milwaukie Downtown and Riverfront Land Use Framework recognizes and builds upon the rich character and history of our town. It celebrates the diversity and spirit of the people and places of Milwaukie, and reinforces those special qualities.

City officials, community leaders and area businesses have known for a long time that Milwaukie's historic downtown, nestled at the edge of the Willamette River, can and should be a vital center for the community's economic and social activity. Starting in 1999, the City of Milwaukie began a process to develop a concept for downtown Milwaukie that reflects the vision of the people who live and work there. During a number of community design forums, citizens have tested various aspects of the plan, with local residents ranking their priorities for the downtown area.



Downtown view from Willamette River

This Land Use Framework represents the leadership of a skilled volunteer Riverfront Board and the input of the more than 2000

community members who have attended meetings, returned surveys, provided focus and ideas, and directed the plan.

Fundamental Concepts

Milwaukie's unique character is at the heart of the Downtown and Riverfront Framework. The Framework reconnects Milwaukie to the Willamette River, knitting together the seam of McLoughlin Boulevard. It creates the new Riverfront Park as the city's "living room." And it calls for revitalizing historic buildings while designing new structures to harmonize with the town's historic character.

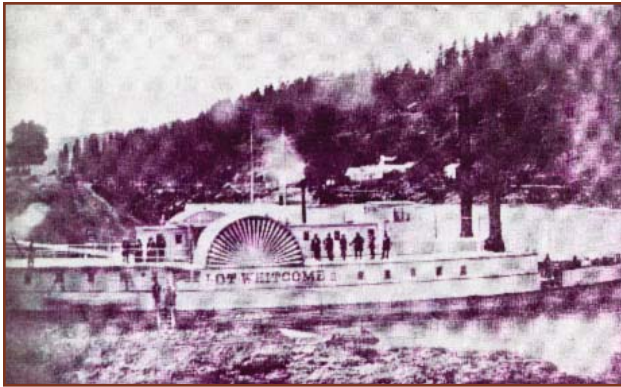


Historic Masonic Lodge

Anchors and attractors are used to build upon existing resources, and to strengthen the Main Street "retail spine." South Downtown is refreshed with a new public plaza and light rail station. Thus the Framework is a blueprint to make Milwaukie and its downtown a vital, livable and sustainable community.

Amenities and Open Spaces

Milwaukie is fortunate to have a setting that inspires its citizens, that offers history, beauty and vitality. The Willamette River, Johnson Creek, and Kellogg Creek all provide beautiful natural borders to the downtown area. The Milwaukie Downtown and Riverfront Framework capitalizes on these natural resources, by restoring the creeks and improving the riverfront for public use. Thus both the town and the natural areas are reinvigorated.



Lot Whitcomb, 1850

Implementation

This Framework coordinates with Milwaukie's existing planning efforts and regulations. The Milwaukie Comprehensive Plan provides the guiding policies to manage the city's physical elements. Ancillary planning documents are part of the Comprehensive Plan. These include the Milwaukie Vision Statement of 1995 and the Town Center Master Plan of 1997. The latter responds to Milwaukie's designation as one of Metro's 2040 Growth Concept Town Centers. The Downtown and Riverfront Framework is a conceptual document. Therefore, various sections have

been adopted as part of, or referenced by the existing Milwaukie Zoning Ordinance, Subdivision Ordinance, or Municipal Code to be effective as a regulatory document.

The Downtown and Riverfront Framework responds to environmental policies including Metro's Titles 3 and 13 requirements and the Federal Endangered Species Act.

This Framework also reflects the adopted South Downtown Concept Plan, which serves as a refinement plan to the area south of Washington Street that will be most directly influenced by the new light rail station.

Phasing and Financing Strategy

The Framework is based on the idea that investing in the future of Milwaukie makes sense. Investment offers employment and opportunity for all the citizens of Milwaukie; it enriches the town literally and figuratively. Smart public improvements stimulate substantial private investment. Thus, investing today in a better quality of life for downtown Milwaukie makes sense for the whole town, for years to come.

1.4 Our Guiding Principles—A Touchstone

The community's vision for downtown was reaffirmed through public outreach in 2013. In 1999, the City of Milwaukie began a process to develop its vision of the downtown and riverfront. Throughout this community effort, the Framework was developed in accordance with and responded to the following guiding principles:

Creating a livable community:

- Provide for residents, workers and visitors alike.
- Provide for people of all ages, cultures, ethnic groups and incomes.
- Provide cultural arts and entertainment facilities.
- Provide significant open spaces and connections to the riverfront.

- Provide for specific “programmatic” requirements, such as parking or visibility from major roadways.

Ensuring economic success:

- Efficiently maximize current investment in infrastructure.
- Spur further private investment.
- Recognize and respond to the current marketplace.
- Establish a strategy for capturing unrealized market niches.
- Complement, protect and promote the continued growth and vitality of current businesses.



1.5 Fundamental Concepts Map



1.6 South Downtown Concept Plan Area



(Illustration from 2011 South Downtown Concept Plan)

1.7 Fundamental Concepts

Anchors and Attractors

The keystone to building a successful downtown is to build upon existing resources - the quality stores and offices that we already have - and supplement these with anchors and attractors - places used by hundreds of people on a daily basis. A grocery store, for example, could generate considerable foot traffic, which would in turn provide additional customers for downtown businesses. A public plaza at the south end of Main Street, as depicted in the South Downtown Concept Plan, will provide gathering and event space, revitalize the area, and complement surrounding commercial uses.

The framework includes key elements which will be necessary to achieve these goals. New "anchor" and "attractor" uses are as follows:

- Village housing opportunity area north of Harrison St.
- New light rail station and public plaza in South Downtown.

The Main Street "Retail Spine"

Reactivating Main Street is a major focus - re-establishing and strengthening a lively storefront retail character with a pedestrian emphasis and 24-hour use.

The framework establishes an environment in which people can shop, work, live and socialize along Main Street. It addresses and repairs the fundamental problems that have drained downtown of its vitality. The Framework suggests adding to or filling in

blocks with new uses and in some instances tearing down buildings and starting over again.

Along Main Street, the fabric of ground floor retail establishments will create a lively flow of pedestrian activity. The retail framework promotes a healthy retail street that includes:

- Retail on both sides of the street.
- Continuous retail facades with no interruptions.
- On-street parking in front of retail.
- Anchor retail at both ends of Main Street.
- Retail on all four corners of intersections.
- A pedestrian loop.
- Safe, signalized pedestrian crossings.
- Pedestrian-friendly amenities - wide sidewalks, landscaping, benches.

This north-south flow of activity will be further enlivened where it intersects with the new Main Street Plaza at Main Street and Adams Street, which provides views of the Willamette River and a location for events and activities such as the Farmers Market.



McLoughlin Blvd Commercial Corridor

Enhancing McLoughlin Blvd as the primary entry point to downtown Milwaukie from the north, south, and riverfront trails provides a sense of place for visitors to downtown and provides a visual connection between downtown and the riverfront.

The framework establishes an environment that is visually attractive and accommodates the pedestrian. Along McLoughlin Blvd, new buildings will provide a comfortable pedestrian environment while welcoming visitors to the riverfront into downtown. The framework promotes a corridor that includes:

- Parking beside or behind buildings
- Continuous "street wall"
- Strong intersections at key signalized crossings
- Gateway treatments at Scott Street and Sparrow Street to make it clear that visitors are entering downtown Milwaukie

21st Ave Mixed Use Corridor

With the construction of the light rail station, 21st Ave is a key pedestrian connection between the station and the bus shelter area on Jackson St. It is also a direct connection to key civic institutions including Milwaukie High School, the Waldorf School, City Hall, and Ledding Library.

The framework establishes an environment that is pedestrian-friendly, vibrant, and attractive, while focusing retail uses on the Main St spine. The 21st Ave corridor

includes:

- Live/work units, residential, and mixed use development
- Pedestrian-friendly amenities - wide sidewalks, landscaping, benches.

Connecting to the River

Riverfront Park will be the location for special events such as "Festival Daze," holiday celebrations and community assemblies. Safe pedestrian and bicycle crossings between downtown and Riverfront Park and multiuse trails are important. Pedestrian connections will be strengthened at key east/west streets to allow pedestrian and bicycle access to the park.

South Downtown Planning Area

The South Downtown Concept Plan refined the vision for the area south of Washington St. The key concepts of the Plan include:

- The development in downtown is fine-grained and human-scale.
- An active, vibrant public plaza provides the central anchor for South Downtown, and provides views to the Willamette River.



- The plaza includes an interactive fountain, seating, and is graded toward the river.
- South Downtown is located in close proximity to several natural areas, including Kellogg Lake, Dogwood Park, Kronberg Park, and Riverfront Park. Access to these natural areas should be provided via walking trails and/or McLoughlin Blvd crossings.

1.8 Key Land Use Features



Revitalizing Main Street and downtown:

- Main Street improvements - shops, services, and family wage jobs
- Save landmarks - to preserve history and heritage
- Pedestrian friendly improvements throughout downtown
- New South Downtown plaza and light rail station



Reconnecting to the River:

- New McLoughlin Bridge - to knit the seam between the downtown and the river
- New Riverfront Park - the City's living room
- New and enhanced pedestrian crossings



Restoring Natural Areas and Parks:

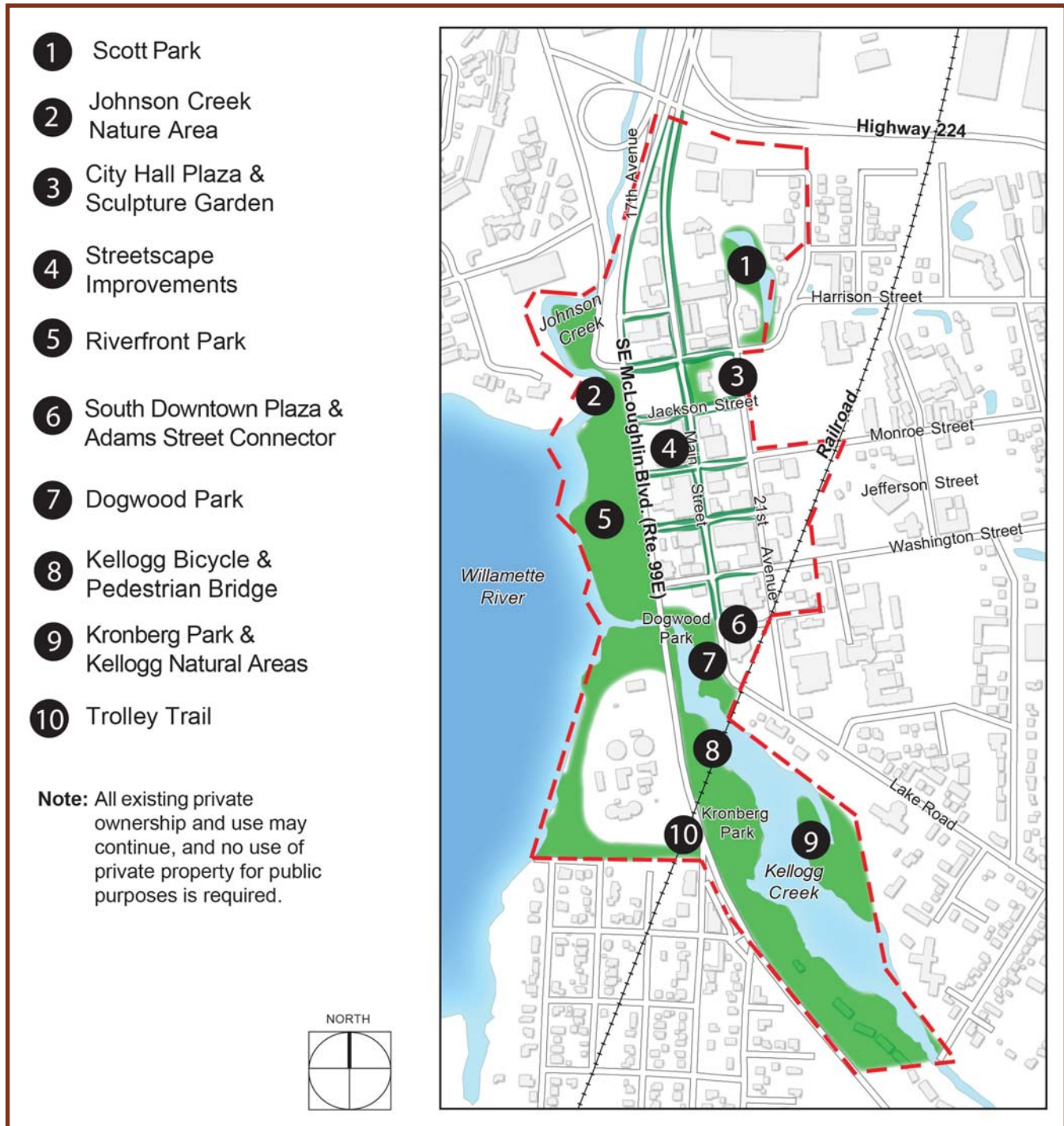
- Restore Spring Creek
- Maintain Johnson Creek restoration
- Continue efforts to restore Kellogg Creek



Providing Quality Housing:

- Provide a variety of quality housing types including rowhouses, apartments and condominiums (both stand-alone and as part of mixed use development) and live/work spaces.

1.9 Amenities and Open Space Framework Map



1.10 Amenities and Open Space Framework

The Milwaukie Downtown and Riverfront Land Use Framework Plan includes a system of outdoor open and green spaces weaving around and through the city. There are creeks, restored wetlands, the Willamette River, and a landscaped urban plaza.

Many of the areas are subject to various municipal, state and federal regulations intended to protect environmental and natural resource values; the elements of this framework are intended to be consistent with those regulations.

① Scott Park

Adjacent to the Ledding Library and North Main Village, Scott Park offers lawn and play areas for children and adults. An improved Scott Lake connects through a natural stream to the restored Crystal Creek.

② Johnson Creek Nature Area

The creek connects the Springwater/Crystal Creek Trail to the north with the Riverfront Park and downtown.

③ City Hall Plaza and Sculpture Garden

The City Hall plaza and Sculpture Garden provide places to stop, rest, and people-watch.

④ Streetscape Improvements

Downtown streets benefit from wider sidewalks, crosswalks, curb extensions,

pedestrian-scaled lighting, street furniture, street trees and planting beds. Main Street is emphasized as the primary retail street with special sidewalk treatments and wider sidewalks allowing for sidewalk café seating and retail displays.

⑤ Riverfront Park

The park is being improved and will become the City's "living room" including a festival lawn, overlooks for viewing the river, pathways for pedestrians and cyclists, improvements to the boat launch and parking, and extensive riverside plantings. Located adjacent to downtown and nestled between the Willamette River, Kellogg Creek and Johnson Creek, Riverfront Park is the focus of Milwaukie's open space framework.

⑥ South Downtown Plaza and Adams Street Connector

Create a major plaza forming the core and focus of the South Downtown. The plaza will have views to the Willamette, retail spaces to the east, connections to natural areas, and Main Street running through the center. During events, the plaza and a portion of Main Street will be closed to vehicular traffic. The Adams Street Connector is a pedestrian green street that provides a pedestrian connection between the light rail station and the South Downtown Plaza.

⑦ Dogwood Park

Dogwood Park is expanded and integrated to the north and east with the South Downtown Plaza and Main Street streetscape improvements, and to the south and west with the Kellogg Nature Area.

⑧ Kellogg Bicycle and Pedestrian Bridge

Connecting the open spaces of Dogwood Park and Kronberg Park and the Trolley Trail beyond with a bicycle/pedestrian bridge.

⑨ Kronberg Park and Kellogg Natural Areas

Planning for improvement of Kronberg Park, and restore the creek and wetlands to a salmon-friendly state. Restoration of Kellogg Creek will provide nature trails through native riparian banks and along the restored stream

⑩ Trolley Trail

The 6-mile Trolley Trail multiuse path stretches between Riverfront Park in downtown Milwaukie to Gladstone to the south.

2. Implementation

2.1 Projects

This diagram sets forth key projects for the Land Use Framework Plan. The first key projects will establish a climate of positive change and growth, while larger or more costly ones would occur later.

